

DRAFT

FHWA-Indiana Environmental Document

CATEGORICAL EXCLUSION LEVEL 1 FORM

GENERAL PROJECT INFORMATION

Road No./County:

Washington Avenue (Ave)/Knox County

Designation Number(s):

2101753

Project

Description/Termini:

Roadway Reconstruction, Washington Ave Phase II Reconstruction from the intersection of Washington Avenue and St. Clair Street (St), extending approximately 2,429 feet linear (ft) to the intersection of Washington Ave and Emison Ave.

☒ X

CE Level 1 documentation for
exempted projects

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Additional Information
to CE Level 1

Approval:

INDOT DE/ESD Signature and Date

Release for Public Involvement:

INDOT DE/ESD Initials and Date

Certification of Public involvement:

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer:

Signature and Date

CE Preparer:

Laura Rogers, SJCA Inc.

Name and Organization

Indiana Department of Transportation

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GENERAL PROJECT INFORMATION, DESCRIPTION, AND DESIGN INFORMATION	
Purpose and Need:	<p>Need:</p> <p>The need for this project is primarily due to existing safety, maintenance, and connectivity issues on Washington Ave in the City of Vincennes. The pavement is predominantly hot mix asphalt (HMA) in fair condition with fatigue cracking, block cracking, and rutting in the wheel paths. The existing pavement section contains remnants of the Old United States (US) 50 Highway roadbed as well as rail ties, bricks, and concrete from the old trolley rail system, which contributes to failing pavement and frequent maintenance repairs (Appendix I3). The existing curb is virtually gone from years of overlay without milling. The existing stormwater system is outdated and often overflows during heavy rainfall, causing flooding on the street and sidewalks creating hazards for pedestrians and drivers. The current existing street parking along Washington Street is non-delineated. The existing sidewalk present on both sides of the roadway is in poor condition and has curb ramps that are not Americans with Disabilities Act (ADA) compliant. No crosswalks are marked. No bicycle facilities currently exist along this section of Washington Ave. The traffic signal at the intersection of St Clair St and Washington Ave is outdated and doesn't meet current standards for traffic signal cantilever structures or signal heads. Washington Ave lane widths at St. Clair St do not meet current design standards. The existing street lighting does not ensure uniform lighting levels from the back of the sidewalk to the center of the roadway.</p> <p>The City of Vincennes adopted a "Complete Streets Program" in 2015, (http://www.vincennes.org/dev/wp-content/uploads/2018/11/Ord.-31-2015-Complete-Streets.pdf) that states that the city shall approach every transportation improvement project as an opportunity to create safer, more accessible streets for all users.</p> <p>Purpose:</p> <p>The purpose of this project is to improve the pavement structure, improve the stormwater drainage system, improve parking, and increase pedestrian accessibility and connectivity with ADA compliance within the project limits. The purpose is also to improve the signal at the intersection of St Clair St and Washington Avenue to current design standards.</p>
Project Description (Preferred Alternative):	<p>The City of Vincennes and the Federal Highway Administration (FHWA) intend to proceed with a roadway reconstruction project involving Washington Ave in the City of Vincennes.</p> <p>Location:</p> <p>This project is located along Washington Ave, from the intersection of Washington Avenue and St. Clair Street to the intersection of Washington Avenue and Emison Avenue, in Vincennes Township, Knox County, Indiana. The roadway is located approximately 0.22 mile southeast of United States (US) 41. A project location map and an aerial map of the project area are included in Appendix B. The project is located in Section 20, Township 3 North, Range 10 West, as shown on the United States Geological Survey (USGS) Vincennes Quadrangle topographic map (Appendix B2).</p>

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	<p>Existing Conditions:</p> <p>Washington Ave is an east-west urban major collector roadway with a posted speed limit of 30 mph from St. Clair St to Ridgeway Ave and 20 mph from Ridgeway Ave to Emison Ave. The corridor is comprised of a 40-ft-wide roadway delineated with only a center double yellow line for most of the project area which provides two travel lanes, one in each direction. Non-delineated street parking is available on each side of the road. At the intersection of Washington Ave and St. Clair St, the roadway includes a through-lane and a dedicated left turn lane in each direction with no street parking available. St. Clair St has 10 ft through and turn lanes. Washington Ave has 12 ft through and turn lanes. The curbs in the project area have deteriorated. Throughout the corridor, stormwater runoff is collected by a storm sewer system with inlets. A five-ft-wide sidewalk is present on both sides of the roadway and is in poor condition with curb ramps that are not ADA-compliant. The sidewalk is separated from the street by an approximately 2.5-foot-wide grass buffer. The pavement is predominantly HMA in fair condition with some fatigue cracking, block cracking, and rutting in the wheel paths. The existing pavement section contains remnants of the Old US 50 Highway roadbed as well as rail ties, bricks, and concrete from the old trolley rail system. There are no existing bicycle facilities present along Washington Ave within the project area. The existing street lighting is non-ornamental, attached to utility poles, and does not ensure uniform lighting levels from the back of the sidewalk to the center of the roadway. The traffic signal at the intersection of St. Clair St and Washington Ave is outdated and doesn't meet current standards for traffic signal cantilever structures. The existing signal heads do not have backplates and are old technology incandescent signals.</p> <p>The project area is located on level terrain and is in an urban area. Land use in the vicinity of the project area is primarily commercial and residential. No existing culverts, bridges, or roadside ditches are present. There are multiple residential and commercial drives located throughout the corridor. There are utility poles located along the north side of Washington Ave. Public utilities are present within and adjacent to the project area, including overhead electric and communication lines and underground communication, water main, sanitary sewer, storm sewer, and gas.</p> <p>A separate Phase I project will provide improvements on Washington Avenue from Emison Avenue to Belle Crossing by reconstructing the roadway, constructing dedicated pedestrian facilities, including bicycle lanes on the roadway, and adding a stormwater sewer system. Construction of Phase I is anticipated in Spring 2024.</p> <p>Preferred Alternative:</p> <p>The preferred alternative involves improving Washington Ave from St. Clair St, extending to the limits of the Phase I project at Emison Ave by reconstructing the roadway, improving dedicated pedestrian facilities, improving lighting, including shared bicycle/vehicular lanes (sharrows) on the roadway, and upgrading the stormwater sewer system. The improved</p>
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	<p>roadway and facilities will provide better access to businesses and neighborhoods for motorists, pedestrians, and bicyclists.</p> <p>This project includes the replacement of the pavement on Washington Ave from St. Clair St to Emison Ave and will consist of milling, widening, and overlay. This project will require approximately one ft of excavation for the removal of the former US Highway 50 roadbed and trolley track remnants. The roadway corridor will be widened from 40 ft to 42 ft from curb face to curb face. Curb and gutter will be added at the approaches of the intersecting minor streets, but the intersecting streets will not be widened, and the intersection radii will remain consistent with what is existing. The proposed lane configuration will consist of one 13 ft sharrow lane for bicycle and vehicular traffic in each direction. There will be eight-ft-wide delineated street parking on both sides of Washington Ave including six ft of pavement width plus two ft of gutter width, except for the intersection approach of Washington Ave and St. Clair St which will include a dedicated left turn lane in each direction with no street parking. All signage and pavement markings will be updated and in accordance with current standards. Refer to Appendix B21-B24 for pavement marking and signing details.</p> <p>The intersection of Washington Ave and St. Clair St will be improved by updating the signal cantilever structures and signal heads to meet current standards. Refer to Appendix B18 for traffic signal details. This intersection will also include storm sewer, sidewalk, and curb ramp improvements as shown in Appendix B14.</p> <p>Drainage of the roadway will be routed through an enclosed storm sewer system that collects the runoff from the roadway in traditional roadway inlets. Storm system design will comply with INDOT standards and City of Vincennes standard practices.</p> <p>The grass buffer separating the street and sidewalks will be removed. The sidewalks in the project area will be widened to six ft. All pedestrian facilities will be constructed following current ADA standards and curb ramps will be added at all intersections. Crosswalks will be added at all four legs of the intersection at the St. Clair St and the State St intersections with Washington Ave. Crosswalks will be added crossing the side streets only at De Wolf St, Carbon St, McKinley Ave, Ridgeway Ave, and Ritterskamp Ave. The crosswalks at the Washington Ave and St. Claire St intersection will feature audible push buttons and pedestrian countdown signals. The utility poles currently located in the grass buffer on the north side of Washington Ave will be relocated north of the new sidewalk. The utility company will be responsible for moving the poles.</p> <p>New lighting will be provided along Washington Avenue within the project limits. The lights to be installed will be an ornamental streetlight with mast arms. The lighting design will ensure uniform lighting levels from the back of the sidewalk to the center of the roadway. Refer to Appendix B19-B20 for lighting details.</p>
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	<p>Utility relocation will be required to allow for construction; these impacts are anticipated to be temporary and will be resolved following project completion. Coordination between the project designer and utility companies is ongoing and will continue throughout the design process.</p> <p>Traffic will be maintained by a two-phase closure of Washington Ave. A detour will be provided for both vehicular and pedestrian traffic. Refer to Appendix B10 and the <i>Maintenance of Traffic (MOT)</i> section of this document for additional information regarding the MOT plan. The project will require 0.45 acre of permanent right-of-way (ROW) and 0.004 acre of temporary ROW. Three trees (approximately 0.001 acre) and 0.39 acre of vegetation will be cleared within the project limits. No permanent or temporary stream or wetland impacts will result from project construction activities. Impacts will be minimized to the greatest extent practicable to complete the project. No mitigation will be necessary for tree clearing or vegetation disturbance.</p> <p>This alternative meets the purpose and need of the project by improving the pavement structure, the stormwater drainage system, and parking. It will increase pedestrian accessibility and connectivity with ADA compliance within the project limits, and the signal at the intersection of St Clair St and Washington Avenue will meet current design standards.</p> <p>The total project length will be approximately 0.46 mile and will include Washington Ave from the intersection of Washington Ave and St. Clair St, extending approximately 2,429 linear ft to the limits of the Phase I project at the intersection of Washington Ave and Emison Ave. These termini will allow for the roadway and sidewalk reconstruction; therefore, the project has logical termini. The project will not rely on any other project for completion; therefore, the project has independent utility.</p>		
Other Alternatives Considered:	<p>No Build/Do Nothing Alternative</p> <p>In the No Build/Do Nothing alternative, no expenditure of funds or construction would occur. However, this alternative does not meet the purpose and need of the project to improve the pavement structure, improve the stormwater drainage system, improve parking, and increase pedestrian accessibility and connectivity with ADA compliance within the project limits; or improve the signal at the intersection of St Clair St and Washington Avenue to current design standards. Therefore, this alternative was discarded from further consideration.</p>		
Funding Source(s):	<input checked="" type="checkbox"/> Federal	<input type="checkbox"/> State	<input checked="" type="checkbox"/> Local <input type="checkbox"/> Other
Project Sponsor:	City of Vincennes		
Estimated Cost:	\$5,102,000 (Fiscal Year (FY) 2027)	Project Length:	Approx. 0.46 mile
Public Involvement:	No: X	Yes:	
Notice of Survey letters were mailed to potentially affected property owners near the project area on November 22, 2022, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Survey letter is included in			

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Appendix G1.

The project does not meet any of the conditions set by the current *Indiana Department of Transportation (INDOT) Project Development Public Involvement Procedures Manual* that require formal public involvement. Therefore, the project sponsor is not required to offer the public an opportunity to request a public hearing. The project is not anticipated to cause any public controversy. This does not preclude the need for public involvement or public information meeting in the future.

Right-of-Way:	No:	Yes: X
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Within the vicinity of the project, the existing ROW runs along the edges of the existing sidewalk of Washington Ave, extending 30 feet from the centerline of Washington Ave on both sides, and consists of roadway, street parking, sidewalk, grass strips, driveways, parking lots, and residential yards. Land use within the vicinity of the existing ROW of Washington Ave includes a mix of residential and commercial uses.

This project will acquire a total of 0.45 acre of permanent ROW. Approximately 0.17 acre of permanent ROW will be acquired from commercial properties and approximately 0.28 acre of permanent ROW will be acquired from residential properties for lighting and utility installation. Proposed ROW widths will extend 35 to 45 feet from the centerline of Washington Ave on both sides. The project also requires 0.004 acre of temporary ROW. Approximately 0.001 acre of temporary ROW will be acquired from commercial properties for grading purposes and approximately 0.003 acre of temporary ROW will be acquired from residential properties for driveway installation. Refer to the plans in Appendix B14-B17 for existing and proposed ROW limits.

Note: The ROW totals were updated subsequent to the mailing of the early coordination letter (Appendix C1).

If the scope of permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Maintenance of Traffic (MOT) During Construction:	No:	Yes: X
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The MOT plan for the project will require a two-phase closure of Washington Ave for the duration of project construction, which is anticipated to be 13 months.

The first phase will require the closure of the Washington Ave and St. Clair St intersection. St. Clair St traffic will be redirected along a detour route utilizing 13th St, Hart St, and North 6th St. This detour will add approximately 1.2 miles and three minutes of additional travel. Meanwhile, Washington Ave traffic will be redirected along a detour route utilizing College Ave, North 6th St, and State St. This detour will add approximately 0.3 mile and one minute of additional travel. These detours will be in place for approximately two months.

During the first phase, the Washington Ave and St. Clair intersection will remain open to pedestrian traffic by closing only one sidewalk corner at a time and routing pedestrians around the closure using pedestrian channelizers and temporary curb ramps. Note: The first phase of the MOT is not currently shown in the plans in Appendix B10, but the final plans will be updated to include both MOT phases.

The second phase will require the closure of Washington Ave from east of the Washington and St. Clair St intersection to Emison Ave. Washington Ave traffic will be redirected along a detour route utilizing St. Clair St, North 6th St, and Niblack Boulevard (Blvd). The detour will add approximately 0.6 mile and two minutes of additional travel. This detour will be in place for approximately 11 months.

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During the second phase, pedestrian traffic will be rerouted along existing sidewalks. Pedestrians on the north side of Washington Ave will be routed down Indiana Ave via St. Clair St and State St. Pedestrians on the south side of Washington Ave will be routed down 11th St via St. Clair St and State St. A short-term closure, seven calendar days, of the sidewalk on Washington Ave from State St to Emison Ave, will be permitted to reconstruct this segment of sidewalk in order to use it for the pedestrian MOT. During all phases, pedestrian traffic will be maintained within the project area by the contractor. Current Standard Drawings and Specifications will be utilized to maintain this access during construction. Please refer to Appendix B10 for additional information regarding the MOT plan for the project.

The closure will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

Bridge(s) and/or Small Structure(s) (include structure number(s)):	No: X	Yes:
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No bridges or small structures are located within the project area.

IDENTIFICATION AND EVALUATION OF IMPACTS

Early Coordination:

Early coordination letters were sent to environmental agencies on June 02, 2023, July 26, 2023, August 3, 2023, and August 24, 2023 (Appendix C1-C2).

Agency	Date Sent	Response/Accessed Date	Appendix
Indiana Geological and Water Survey (IGWS) (Accessed Online)	June 02, 2023	June 02, 2023	C3
Natural Resources Conservation Service (NRCS)	July 26, 2023	August 3, 2023	C4
Indiana Department of Environmental Management (IDEM) Ground Water Section	July 26, 2023	August 4, 2023	C5-C6
Indiana Department of Natural Resources Division of Fish & Wildlife (IDNR-DFW)	July 26, 2023	August 25, 2023	C7-C9
Federal Highway Administration (FHWA)	July 26, 2023	No Response	N/A
Knox County Highway Department	July 26, 2023	No Response	N/A
Knox County Commissioners	July 26, 2023	No Response	N/A
National Park Service (NPS)	July 26, 2023	No Response	N/A
Knox County Highway Department	July 26, 2023	No Response	N/A
Knox County Surveyor	July 26, 2023	No Response	N/A
City of Vincennes Parks and Recreation	July 26, 2023	No Response	N/A
U.S. Department of Housing & Urban Development (HUD)	July 26, 2023	No Response	N/A
U.S. Fish and Wildlife Service (USFWS)	July 26, 2023	No Response	N/A
INDOT Project Manager	July 26, 2023	No Response	N/A
INDOT Vincennes District Environmental	July 26, 2023	No Response	N/A
Knox County Emergency Management	July 26, 2023	No Response	N/A
Vincennes Community School	July 26, 2023	No Response	N/A

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Corporation (VCSC)			
Washington Elementary School	July 26, 2023	No Response	N/A
City of Vincennes Mayor's Office	July 26, 2023	No Response	N/A
Vincennes Fire Department	July 26, 2023	No Response	N/A
Vincennes Police Department	July 26, 2023	No Response	N/A
Civitan Childrens Center	August 3, 2023	No Response	N/A
Vincennes Municipal Separate Storm Sewer System (MS4) Coordinator	August 24, 2023	No Response	N/A

All applicable recommendations are included in the *Environmental Commitments* section of this Categorical Exclusion (CE) document.

Streams, Rivers, and Other Jurisdictional Features Impacted:	No: X	Yes:
Based on the desktop review, the aerial map of the project area (Appendix B3), and the Red Flag Investigation (RFI) report (Appendix E1-E12), there are four stream segments, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There are no streams, rivers, watercourses, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visit on May 25, 2023, by SJCA Inc. Therefore, no impacts are expected. No Federal, Wild, and Scenic Rivers; State Natural, Scenic, and Recreational Rivers; Outstanding Rivers for Indiana; navigable waterways, or National Rivers Inventory waterways are present within or adjacent to the project area.		

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Open Water Feature(s):	No: X	Yes:
Based on the desktop review, the aerial map of the project area (Appendix B3), and the RFI report (Appendix E1-E12), there are no open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on May 25, 2023, by SJCA Inc. Therefore, no impacts are expected.		

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Wetlands:	No: X	Yes:
Based on the desktop review, the aerial map of the project area (Appendix B3), and the RFI report (Appendix E1-E12), there are nine wetlands within the 0.5-mile search radius. There are no wetlands within or adjacent to the project area, which was confirmed by the site visit on May 25, 2023, by SJCA Inc. Therefore, no impacts are expected.		

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Terrestrial Habitat:	No:	Yes: X
Based on a desktop review, the site visit on May 25, 2023, by SJCA Inc., and the aerial map of the project area (Appendix B3), terrestrial habitat exists within the project area in the form of maintained roadside and lawns. Vegetation within the roadside ROW was characterized as upland grasses, lawn weeds, and landscaping trees and shrubs that are common in southwestern Indiana. Three trees (approximately 0.001 acre) are anticipated to be removed for this project. All three trees are unsuitable summer bat habitat because they are single, ornamental trees within an urban area; and there will be no		

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federal time of year restrictions on removal. The species of trees to be removed are red maple (*Acer rubrum*), silver maple (*Acer saccharinum*), and Japanese cherry (*Prunus serrulata*). Approximately 0.39 acre of vegetation, consisting mostly of lawn grasses, will be cleared within the project limits for roadway and sidewalk reconstruction. Impacts will be minimized to the greatest extent practicable to complete the project by completing as much work as possible from the existing roadway footprint. Mitigation is not anticipated to be necessary for tree clearing and vegetation disturbance.

The IDNR-DFW responded to early coordination on August 25, 2023, with recommendations to mitigate for unavoidable habitat impacts, to use appropriate lighting, to revegetate all bare and disturbed areas as soon as possible after project completion, to minimize and contain tree and brush clearing to the project limits, to use appropriately designed measures for sediment and erosion control, and to minimize the entrapment of wildlife by using appropriate erosion control blankets (Appendix C7-C9). All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Protected Species:	No:	Yes: X
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Based on a desktop review and the RFI report (Appendix E1-E12), completed by SJCA Inc. on August 22, 2023, the IDNR Knox County Endangered, Threatened, and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated August 25, 2023, the Natural Heritage Program's Database has been checked and no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity. An INDOT 0.5-mile bat review occurred on April 4, 2023, and did not indicate the presence of endangered bat species in or within 0.5 mile of the project area.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C20-C32). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and northern long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were generated in the IPaC species list along with the Indiana bat and NLEB. Refer to paragraph below.

The official species list generated from IPaC indicated three other species present within the project area. The monarch butterfly (*Danaus plexippus*) is a candidate for listing on the federal endangered, threatened, and rare species list. Additionally, the whooping crane (*Grus americana*) is listed as a non-essential experimental population and the tricolored bat (*Perimyotis subflavus*) is listed as proposed endangered. As candidate, proposed, and experimental species, the monarch butterfly, tricolored bat, and whooping crane are not given any statutory protection under the Endangered Species Act. Therefore, no further coordination is required. The project qualifies for the most current INDOT/USFWS agreement. No further coordination is needed with USFWS at this time.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on August 9, 2023, and based on the responses provided, the project was found to "May Affect – Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C10 – C19). INDOT reviewed and verified the effect finding on August 9, 2023, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Minimization Measures (AMMs) were generated for the project regarding permanent and temporary lighting and ensuring workers are aware of environmental commitments. AMMs and/or commitments are included as firm commitments in the *Environmental Commitments* section of this document.

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This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources:

No: X

Yes:

Based on a desktop review and the Indiana Karst Region map, the project is located in the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the USGS topographic map of the project area (Appendix B2) and the RFI report (Appendix E1-E12), there are no karst features identified within or adjacent to the project area. In the early coordination response dated June 2, 2023, the IGWS did not indicate that karst features exist in the project area (Appendix C3). The IGWS automated response did indicate potential mine subsidence, high liquefaction potential, a 1% annual chance flood hazard, a low potential for bedrock resources, a high potential for sand and gravel resources, and underground coal mines. The features will not be affected because no bedrock, sand or gravel extraction sites, or mines are known to occur within the project area. Response from IGWS has been communicated with the designer on June 2, 2023. No impacts are expected.

Drinking Water Resources:

No:

Yes: X

Sole Source Aquifer

The project is located in Knox County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/Environmental Protection Agency (EPA)/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and Source Water

In an early coordination letter dated August 4, 2023, IDEM stated the project is not located within a Wellhead Protection Area or Source Water Area (Appendix C5-C6). No impacts are expected.

Water Wells

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on August 24, 2023, by SJCA Inc. There is one water well located approximately 350 feet south of the project area. The feature will not be affected because no project construction activities will occur within the vicinity of the well. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that this well will be affected, a cost to cure will likely be included in the appraisal to restore the well.

Urban Area Boundary

Based on a desktop review of the MS4 Boundaries Map for Indiana (<https://www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/>) by SJCA Inc. on August 24, 2023, this project is located in an Urban Area Boundary (UAB). An early coordination letter was sent on August 24, 2023, to the Vincennes MS4 coordinator. The MS4 coordinator did not respond within the 30-day time frame. Utility coordination for this project is ongoing and includes the City of Vincennes, the project sponsor. The project includes storm water system improvements that will comply with the requirements and conditions of the Municipal Separate Storm Sewer General Permit (MS4GP). Therefore, no impacts are expected.

Public Water System

Based on a desktop review, a site visit on May 25, 2023, by SJCA Inc., the aerial map of the project area (Appendix B3), and the project plans (Appendix B7 – B24), this project is located where there is a

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public water system. Minor impacts to the public water system are expected during construction including pipe relocations as required. Utility Coordination has been initiated and is ongoing.		
Floodplains:	No: X	Yes:
The IDNR Indiana Floodway Information Portal website (https://www.in.gov/dnr/water/surface-water/indiana-floodplain-mapping/indiana-floodplain-information-portal/) was accessed on August 24, 2023, by SJCA Inc. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F1). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.		
Farmland:	No: X	Yes:
Based on a desktop review, a site visit on May 25, 2023, by SJCA Inc., the aerial map of the project area (Appendix B3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on July 26, 2023, to NRCS. The NRCS indicated the project will not cause a conversion of prime farmland in an August 1, 2023 early coordination response letter (Appendix C4).		
Cultural Resources:	No: X	Yes:
<p>*The CE is being submitted before MPPA Approval- The section will be updated before CE approval.</p> <p>On DATE, the INDOT Cultural Resource Office (CRO) determined that this project falls within the guidelines of Category A, Types 4 and 5, and Category B, Type 1 under the Minor Projects Programmatic Agreement (MPPA) (Appendix D1-D6). MPPA Category A, Type 4 covers roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required. MPPA Category A, Type 5 covers repair, in-kind replacement or upgrade of existing lighting, signals, signage, and other traffic control devices in previously disturbed soils. MPPA Category B, Type 1 covers the replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking. Project activities will occur within undisturbed soils; therefore, an archaeological investigation was completed by SJCA Inc. The Phase Ia archaeological survey determined that there were no archaeological sites in the vicinity of the project, and no further investigation was recommended (Smith 2023). No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.</p>		
Section 4(f) and Section 6(f) Resources:	No: X	Yes:
<p>Section 4(f)</p> <p>Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife/waterfowl refuges, and National Register of Historic Places (NRHP) eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.</p> <p>Based on a desktop review, the aerial map of the project area (Appendix B3), and the RFI report (Appendix E1-E12), there are 11 potential 4(f) resources located within the 0.5-mile search radius. One recreational facility, Washington Elementary School, is described as adjacent to the project area in the RFI report; however, the size of the project area was reduced since the RFI report, and the facility is not</p>		

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adjacent to the current project limits. According to additional research and by the site visit on May 25, 2023, by SJCA Inc., there are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

An early coordination letter was sent on July 26, 2023, to Washington Elementary School; however, no response was received within the 30-day time frame.

Section 6(f)

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of four properties in Knox County (Appendix I1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

Air Quality:	No: X	Yes:
---------------------	--------------	-------------

STIP/TIP

This project is included in the Fiscal Year (FY) 2024-2028 Statewide Transportation Improvement Program (STIP) (Appendix H1).

Attainment Status

This project is located in Knox County, which is currently in attainment for all criteria pollutants according to the US EPA's Nonattainment Areas for Criteria Pollutants Green Book (<https://www.epa.gov/green-book>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

Community Impacts:	No: X	Yes:
---------------------------	--------------	-------------

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. This project will have no relocations and will require less than 0.5 acre of additional permanent ROW; therefore, an EJ analysis is not required per the current INDOT Categorical Exclusion Manual.

No negative reasonably foreseeable impacts are anticipated as a result of this project. This project will not result in changes in the pattern of land use, population density, or the growth rate of the area. The project will not result in indirect effects on air, water, or natural systems (direct community impacts and direct effects on air, water, and natural systems have been discussed in previous sections of this document). This project will not result in incremental impacts to the environment, nor will it result in cumulative impacts from collectively significant actions taking place over a period of time.

The preferred alternative is consistent with the Vincennes Comprehensive Plan and local land use plans developed for the City of Vincennes. As a result, the project is expected to comply with the local/regional development patterns for the area. It was determined that Knox County has an approved

Indiana Department of Transportation

County Knox Route Washington Ave Des. No. 2101753

ADA Transition plan which is attached to Commissioners Ordinance No. 4-2013, adopted March 20, 2013. The project will improve ADA accessibility along the project corridor. The project will comply with and advance the ADA Transition plan.

The City of Vincennes "Complete Streets Program", (<http://www.vincennes.org/dev/wp-content/uploads/2018/11/Ord.-31-2015-Complete-Streets.pdf>) adopted in 2015, states that the city shall approach every transportation improvement project as an opportunity to create safer, more accessible streets for all users. This project advances that program by adding sharrows, increasing ADA accessibility, and improving the intersection at Washington Ave and St. Claire St.

Public Facilities and Services (e.g. schools, emergency services):	No:	Yes: X
---	------------	---------------

Based on a desktop review, the aerial map of the project area (Appendix B3), and the RFI report (Appendix E1-E12), there are nine religious facilities, four schools, nine recreational facilities, one pipeline, ten railroad segments, and two trails mapped within the 0.5-mile search radius. One recreational facility/school, Washington Elementary School, is described as adjacent to the project area in the RFI report; however, the size of the project area was reduced since the RFI report was approved, and the facility is not adjacent to the current project limits. There are no public facilities mapped within or adjacent to the project area, which was confirmed by the site visit on May 25, 2023, by SJCA Inc. Therefore, no impacts are expected. Access to all properties will be maintained during construction.

Based on the site visit on May 25, 2023, by SJCA Inc. and the project plans (Appendix B7 – B24), multiple public utilities are present within the project area, including underground telephone, water, gas, storm and sanitary sewer, and overhead electric and overhead communication lines. Temporary and permanent utility relocation will be required to allow for construction; these impacts are anticipated to be temporary and will be resolved following project completion. Coordination between the project designer and utility companies is ongoing throughout the design process.

Community travel times to and from nearby religious facilities, schools, and recreation facilities will be impacted by the project's road closure and detour, resulting in longer travel times. These impacts are minor and temporary in nature and will resolve once construction is concluded.

Early Coordination was sent to the Washington Elementary School and VCSC on July 26, 2023. Early Coordination was also sent to the privately owned Civitan Childrens Center on August 3, 2023. No comments were received from any of the properties. The designed action will not adversely impact the activities or features of Washington Elementary School, VCSC, or Civitan Childrens Center. All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Hazardous Materials and Regulated Substances:	No:	Yes: X
--	------------	---------------

Based on a review of Geographic Information Systems (GIS) and available public records, the RFI was completed by SJCA Inc. on August 22, 2023 and INDOT Site Assessment and Management (SAM) provided their concurrence on August 24, 2023 (Appendix E1-E12). Seven Resource Conservation and Recovery Act (RCRA) Generator/Treatment Storage and Disposal (TSD) Facilities, one State Cleanup Site, 14 Underground Storage Tank (UST) Sites, 15 Leaking Underground Storage Tank (LUST) Sites, one Waste Transfer Site, one Brownfield, three Institutional Controls, and two National Pollutant Discharge Elimination System (NPDES) Facilities are located within 0.5 mile of the project area. One RCRA Generator/TSD Facility and two LUST Sites are located in, or could affect, the project area.

Indiana Department of Transportation

County Knox Route Washington Ave Des. No. 2101753

RCRA Generator/TSD: Former Fields Cleaners is located 0.03 mile west of the project area. This site is an active dry cleaner that is currently in IDEM's Voluntary Remediation Program (VRP). Chlorinated solvent contamination remains on site in soils and groundwater contamination extends southwest off the site. If excavation occurs in this area, it is possible chlorinated solvent contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

LUST Site: Swifty Service Station #192, 1842 Washington Ave, AI ID#31441, is located within the project area and was formerly the site of a gas station. IDEM issued a No Further Action Determination Pursuant to 1994 UST Guidance on July 6, 2010, but the entire letter is not viewable. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater will be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

LUST Site: Bus Barn/Maintenance Shop, 2120 Washington Ave, AI ID #34277, is located adjacent to the project area. IDEM issued a No Further Action (NFA) Determination Pursuant to 1994 UST Branch Guidance for the site on January 18, 2006. Soil and groundwater petroleum contamination above the 1994 Guidance Corrective Action Guidelines remained on site. If excavation occurs in this area, it is possible chlorinated solvent contamination may be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Further investigation for hazardous material concerns is not required at this time.

Permits:	No:	Yes: X
<p>This project will not impact any streams or wetlands; therefore, the project is not anticipated to require an IDEM Section 401 and USACE Section 404 permit. Because the project is not within a floodway, an IDNR Construction in a Floodway (CIF) permit will not be required. A Construction Stormwater General Permit (CSGP) through IDEM is required because the project will disturb more than one acre in area. No mitigation is anticipated to be required for this project.</p> <p>Applicable recommendations provided by resource agencies are included in the <i>Environmental Commitments</i> section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.</p> <p>It is the responsibility of the project sponsor to identify and obtain all required permits.</p>		

ENVIRONMENTAL COMMITMENTS:

Firm

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT ESD and the INDOT Vincennes District Environmental Section will be contacted immediately. (INDOT ESD and INDOT Vincennes District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)

Indiana Department of Transportation

County Knox Route Washington Ave Des. No. 2101753

3. RCRA Generator/TSD: Former Fields Cleaners is located 0.03 mile west of the project area. This site is an active dry cleaner that is currently in IDEM's Voluntary Remediation Program (VRP). Chlorinated solvent contamination remains on site in soils and groundwater contamination extends southwest off the site. If excavation occurs in this area, it is possible chlorinated solvent contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)
4. LUST Site: Swifty Service Station #192, 1842 Washington Ave, AI ID#31441, is located within the project area and was formerly the site of a gas station. IDEM issued a No Further Action Determination Pursuant to 1994 UST Guidance on July 6, 2010, but the entire letter is not viewable. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater will be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)
5. LUST Site: Bus Barn/Maintenance Shop, 2120 Washington Ave, AI ID #34277, is located adjacent to the project area. IDEM issued a No Further Action (NFA) Determination Pursuant to 1994 UST Branch Guidance for the site on January 18, 2006. Soil and groundwater petroleum contamination above the 1994 Guidance Corrective Action Guidelines remained on site. If excavation occurs in this area, it is possible chlorinated solvent contamination may be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination. (INDOT SAM)
6. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
7. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
8. Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
9. The final plans will be updated to include both MOT phases. (INDOT ESD)
10. Pedestrian traffic will be maintained within the project area by the contractor. Current Standard Drawings and Specifications will be utilized to maintain this access during construction. (INDOT ESD)

For Consideration

11. Always choose fully shielded fixtures that emit no light upward. The Division further recommends that light-shielding be used in forested areas to contain the light to the road corridor and avoid casting light beyond into forested habitat. (IDNR-DFW)

Indiana Department of Transportation

County Knox Route Washington Ave Des. No. 2101753

12. Use “warm-white” or filtered LEDs (CCT < 3,000 K; S/P ratio < 1.2) to minimize harmful blue light emission. (IDNR-DFW)
13. Look for products with adaptive controls like dimmers, timers, and motion sensors. (IDNR-DFW)
14. Consider dimming or turning off lights during non-peak overnight hours. (IDNR-DFW)
15. Avoid the temptation to over-light because of the higher luminous efficiency of LEDs. (IDNR-DFW)
16. Only light the exact space and in the amount required for particular tasks. (IDNR-DFW)
17. If erosion control blankets are used, they shall be heavy-duty, biodegradable, and net free or use loose-woven/Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer’s recommendations for selection and installation); seed and apply mulch on all other disturbed areas. (IDNR-DFW)
18. Do not cut any trees suitable for Indiana Bat or Northern Long-eared Bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30. (IDNR-DFW)

DES 2101753 CE-1 APPENDICES

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Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations⁶	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁷)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁸
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁹
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ¹⁰
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹¹
Approval Level <ul style="list-style-type: none"> District Env. (DE) Env. Serv. Div. (ESD) FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

⁷ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁸ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower-level CE.

⁹ Potential for causing a disproportionately high and adverse impact.

¹⁰ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹¹ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

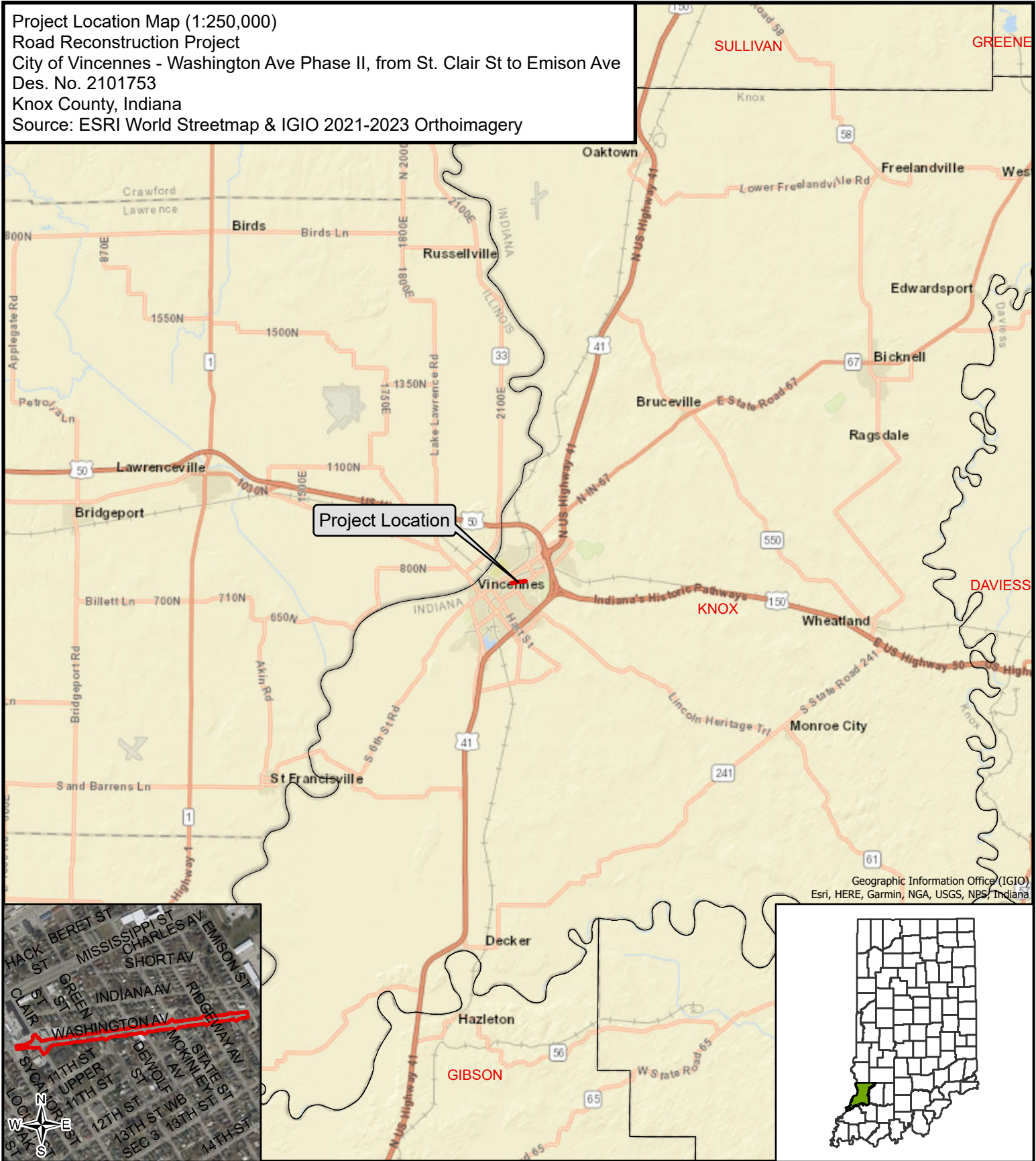
* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

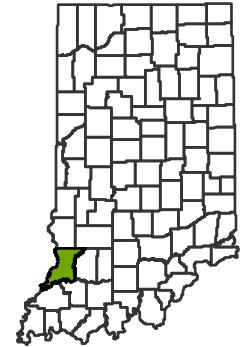
Appendix B

Graphics

Project Location Map (1:250,000)
 Road Reconstruction Project
 City of Vincennes - Washington Ave Phase II, from St. Clair St to Emison Ave
 Des. No. 2101753
 Knox County, Indiana
 Source: ESRI World Streetmap & IGIO 2021-2023 Orthoimagery



Geographic Information Office (IGIO)
 Esri, HERE, Garmin, NGA, USGS, NPS, Indiana



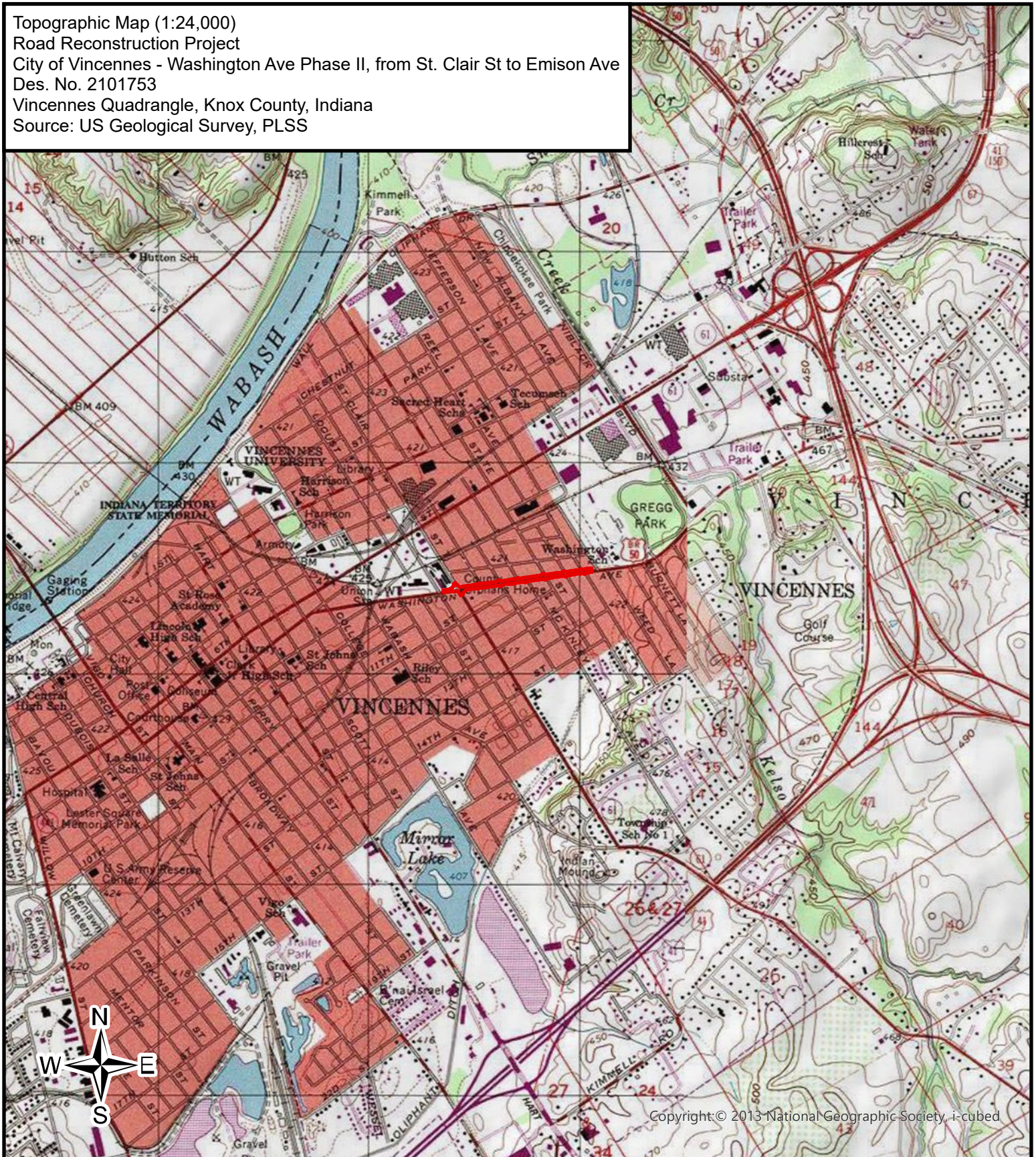
0 3 6
 Miles

Project Location
 Project County



2/6/2024

Topographic Map (1:24,000)
 Road Reconstruction Project
 City of Vincennes - Washington Ave Phase II, from St. Clair St to Emison Ave
 Des. No. 2101753
 Vincennes Quadrangle, Knox County, Indiana
 Source: US Geological Survey, PLSS



Copyright: © 2013 National Geographic Society, i-cubed

0 0.28 0.55
 Miles

Project Location



2/6/2024

Aerial Map (1:2,929)
Road Reconstruction Project
City of Vincennes - Washington Ave Phase II, from St. Clair St to Emison Ave
Des. No. 2101753
Knox County, Indiana
Source: NAIP 2018 Imagery



National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), GIS, Indiana Spatial Data Portal

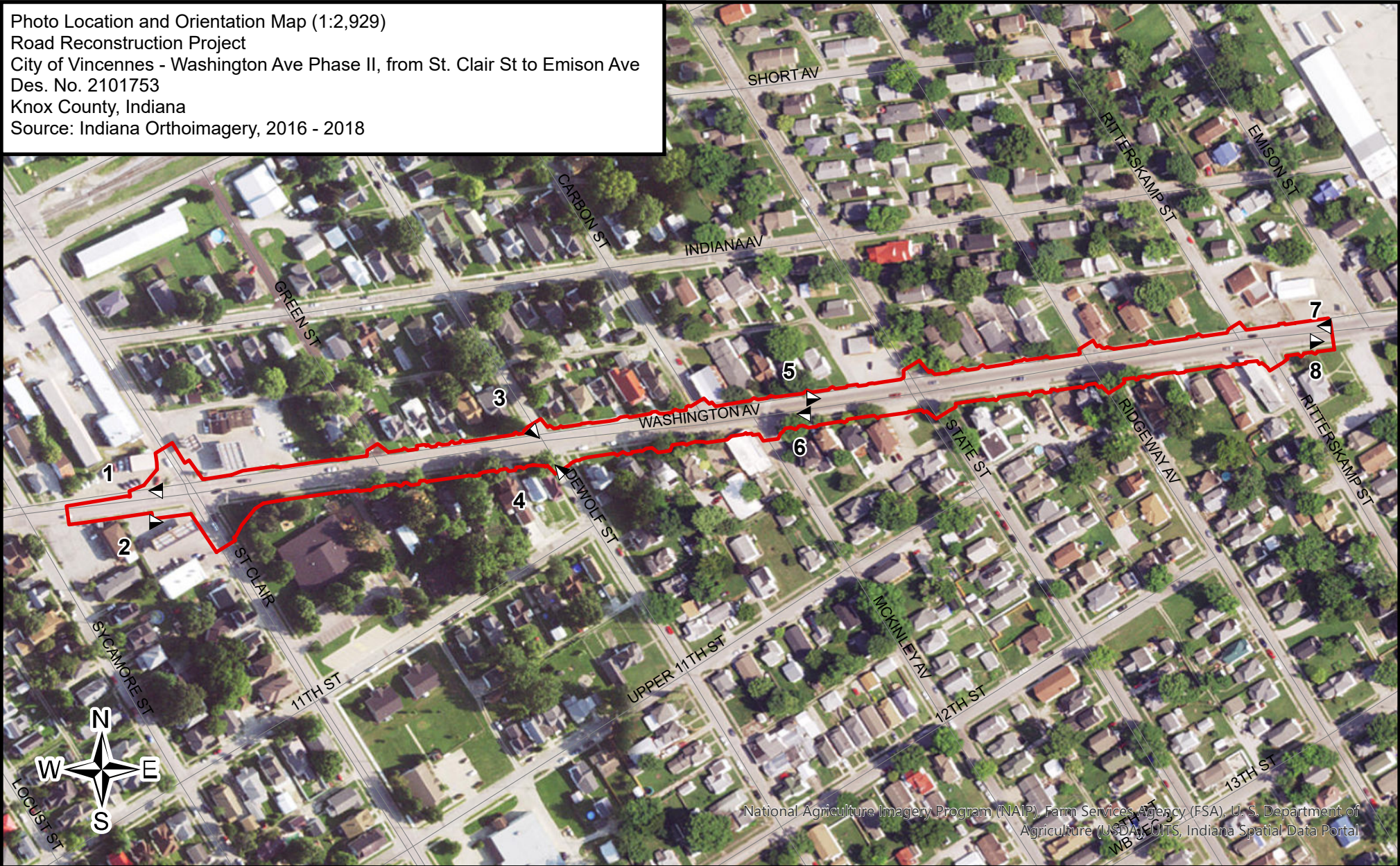
0 190 380
Feet

Project Location



2/6/2024

Photo Location and Orientation Map (1:2,929)
Road Reconstruction Project
City of Vincennes - Washington Ave Phase II, from St. Clair St to Emison Ave
Des. No. 2101753
Knox County, Indiana
Source: Indiana Orthoimagery, 2016 - 2018



National Agriculture Imagery Program (NAIP), Farm Services Agency (FSA), U. S. Department of Agriculture (USDA), GIS, Indiana Spatial Data Portal

0 0.04 0.07
Miles

Project Location
Photo Location



2/6/2024



Photo 1. Facing west at the intersection of Washington Ave and St. Clair St, on the north side of Washington Ave.



Photo 3. Facing southeast at the intersection of Washington Ave and Dewolf St, on the north side of Washington Ave.



Photo 2. Facing east at the intersection of Washington Ave and St. Clair St, on the south side of Washington Ave.



Photo 3. Facing north at the intersection of Washington Ave and Dewolf St, on the south side of Washington Ave.



Photo 5. Facing east along the north side of Washington Ave.



Photo 7. Facing west near the intersection of Washington Ave and Ritterskamp St, on the north side of Washington Ave.



Photo 6. Facing west along the south side of Washington Ave.



Photo 8. Facing east near the intersection of Washington Ave and Ritterskamp St, on the south side of Washington Ave.

PROJECT	DESIGNATION
2101753	2101753
CONTRACT	BRIDGE FILE
R-44285	N/A

CITY OF VINCENNES

ROAD PLANS

WASHINGTON AVENUE PHASE II

PROJECT NO.

2101753

CONST.

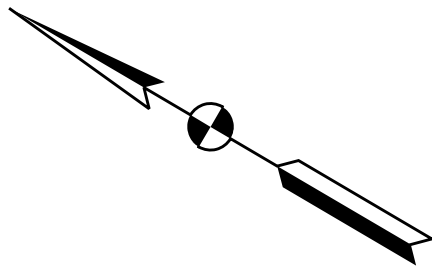
NOTE TO REVIEWER:

CITY SIGNATURE BLOCK WILL BE

ADDED AT A LATER SUBMITTAL

Project Description:

This Roadway Reconstruction Project is located on Washington Avenue, from St. Clair Street to Emison Avenue, approximately 0.22 Miles Southeast of US 41 in Section Survey 20, T 3 N, R 10 W, Vincennes Township, Knox County, Indiana.



Scale: 1" = 1000'



PROJECT LOCATION SHOWN BY

KNOX COUNTY

LATITUDE: 38° 40' 55" N

LONGITUDE: 37° 30' 45" W

Gross Length: 0.46 MI.

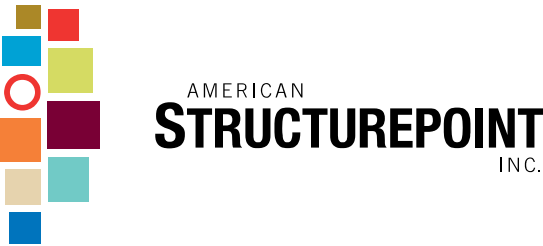
Net Length: 0.46 MI.

Maximum Grade: 1.19 %

Hydrologic Unit Code: 05120113020020

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2024
TO BE USED WITH THESE PLANS.

TRAFFIC DATA		
A.A.D.T.	(2027)	9,900 V.P.D.
A.A.D.T.	(2047)	17,900 V.P.D.
D.H.V	(2047)	1,790 V.P.H.
DIRECTIONAL DISTRIBUTION		58 %
TRUCKS		13 % A.A.D.T.
		N/A % D.H.V.
DESIGN DATA		
DESIGN SPEED		30 M.P.H.
PROJECT DESIGN CRITERIA	RECONSTRUCTION (NON-FREEWAY)	
FUNCTIONAL CLASSIFICATION	LOCAL AGENCY COLLECTOR	
RURAL/URBAN	URBAN (INTERMEDIATE)	
TERRAIN		LEVEL
ACCESS CONTROL		NONE



9025 RIVER ROAD, SUITE 200
INDIANAPOLIS, IN 46240
TEL 317.547.5580 FAX 317.543.0270
www.structurepoint.com

PLANS
PREPARED BY: American Structurepoint, Inc. (317) 547-5580
PHONE NUMBER

CERTIFIED BY: DATE

BRIDGE FILE	
N/A	
DESIGNATION	
2101753	
SHEETS	
SURVEY BOOK	1 of 52
N/A	
CONTRACT	PROJECT
R-44285	2101753

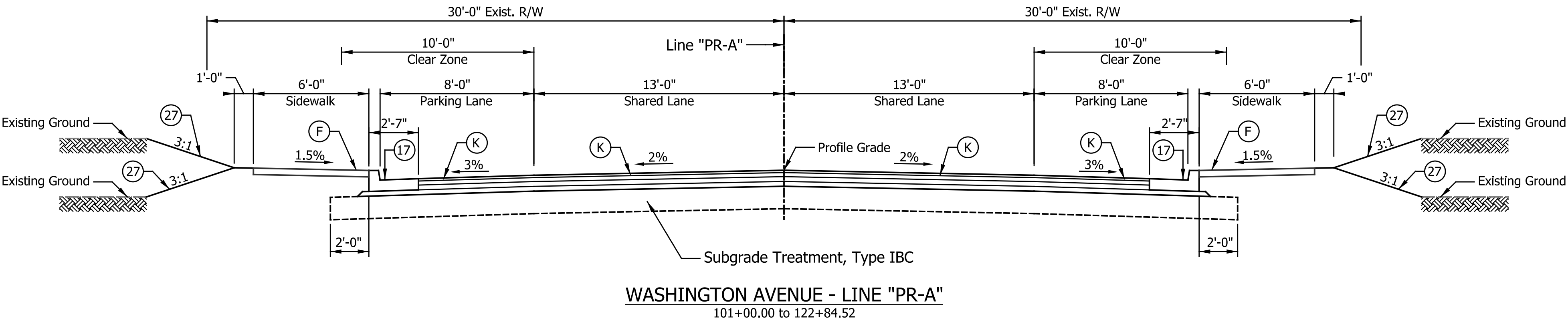
UTILITIES		
CABLE AT&T Distribution 13630 Lorain Avenue Cleveland, OH 44111 (216) 543-0256 David Blackwell g09871@att.com Cableone - Sparklight 500½ Center Cross Edinburgh, IN 46164 (812) 272-2348 George McCormack george.mccormack@cableone.biz ELECTRIC Duke Energy (317) 776-5320 Don Mcduffy dei-dline-coord@duke-energy.com	FIBER Metro Fibernet 3701 Communications Way Evansville, IN 47715 (812) 213-1378 Korie Nellis korie.nellis@metronetinc.com Windstream 5020 Smythe Drive Evansville, IN 47715 (217) 876-7194 Mark Mills mark.mills@windstream.com WATER/STORM WATER/SANITARY SEWER Vincennes Water Utilities 403 Busseron Street Vincennes, IN 47591 (812) 882-5326 Hunter Pinnell hunter@vinutilities.com	GAS Centerpoint Energy 16000 Allisonville Road Noblesville, IN 46061 (765) 287-2119 Jon Eastham publicproject@centerpointenergy.com

GENERAL NOTES	
**	All earth shoulders, median areas, and cut and fill slopes shall be plain or mulch seeded except where sodding is specified
	The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.
	The paper relocation will be cross sectioned by the Engineer before construction.
	Existing asphalt pavement located outside the construction limits, between Sta. _____ and Sta. _____, shall be removed as directed.
	The quantity of peat excavation shown on the plans has been estimated on the basis of theoretical cross sections by using treatment of existing fills, treatment by removal, or treatment by displacement, where each treatment applies.
	All limited access right-of-way (L.A. R/W) is to be fenced with chain link type fence (CLTF) or farm field type fence (FFTF) where specified in the plans.
	Contractor shall verify existing flowline elevations to set the appropriate sump depth.

** REPRESENTS GENERAL NOTES REQUIRED

INDEX	
SHEET NO.	DRAWING INDEX
1	TITLE
2	INDEX AND GENERAL NOTES
3	TYPICAL SECTIONS
4-5	PLAT NO. 1
6	GEOMETRIC TIE-UP SHEET
7	DETOUR PLAN
8-10	PLAN & PROFILES
11-14	CONSTRUCTION DETAILS
15	TRAFFIC SIGNAL DETAILS
16-17	LIGHTING DETAILS
18-22	PAVEMENT MARKING AND SIGNING DETAILS
23-24	APPROACH TABLE
25-26	STRUCTURE DATA TABLE
27-52	CROSS SECTIONS

REVISIONS		
SHEET NO.	DATE	REVISED

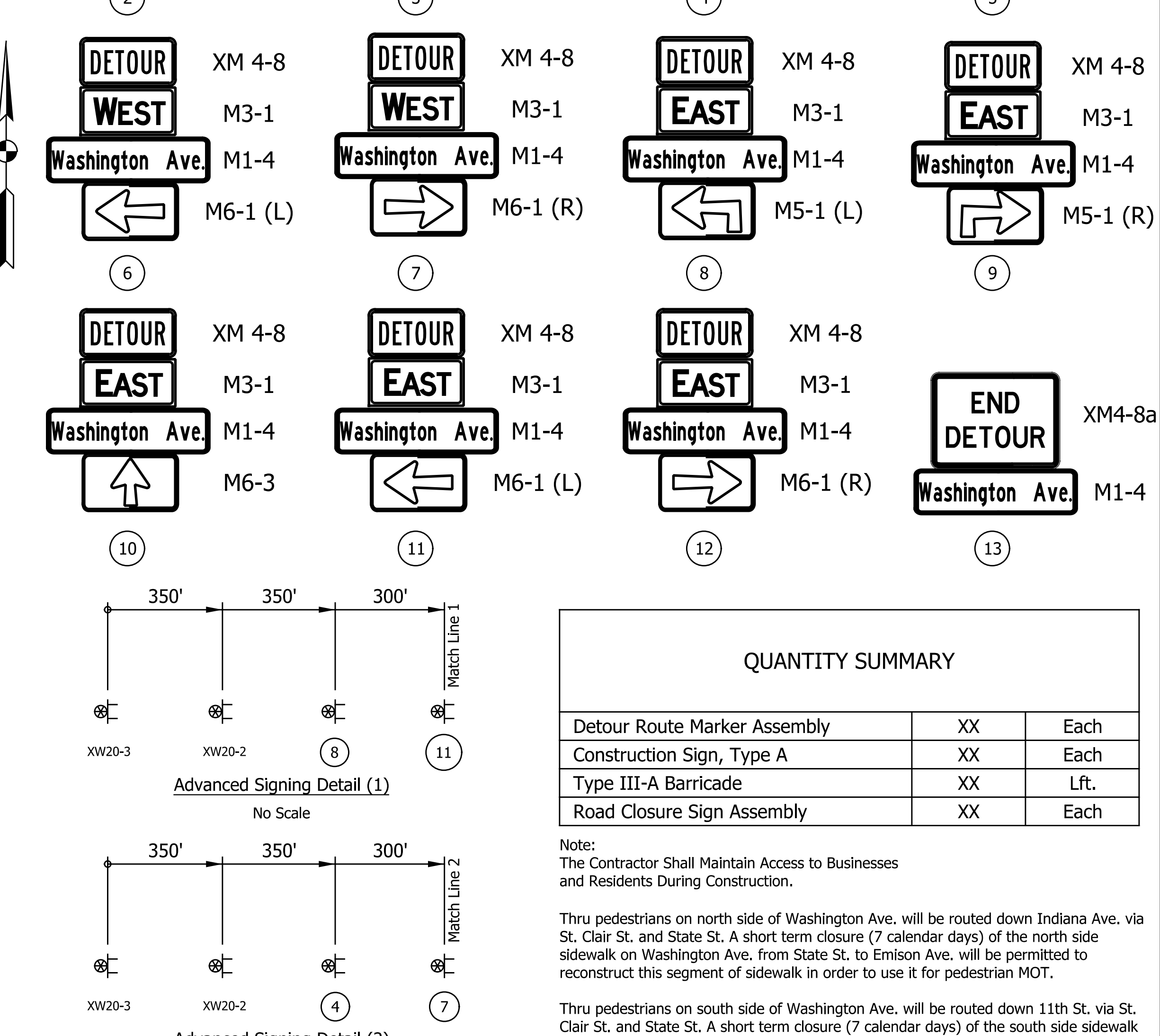
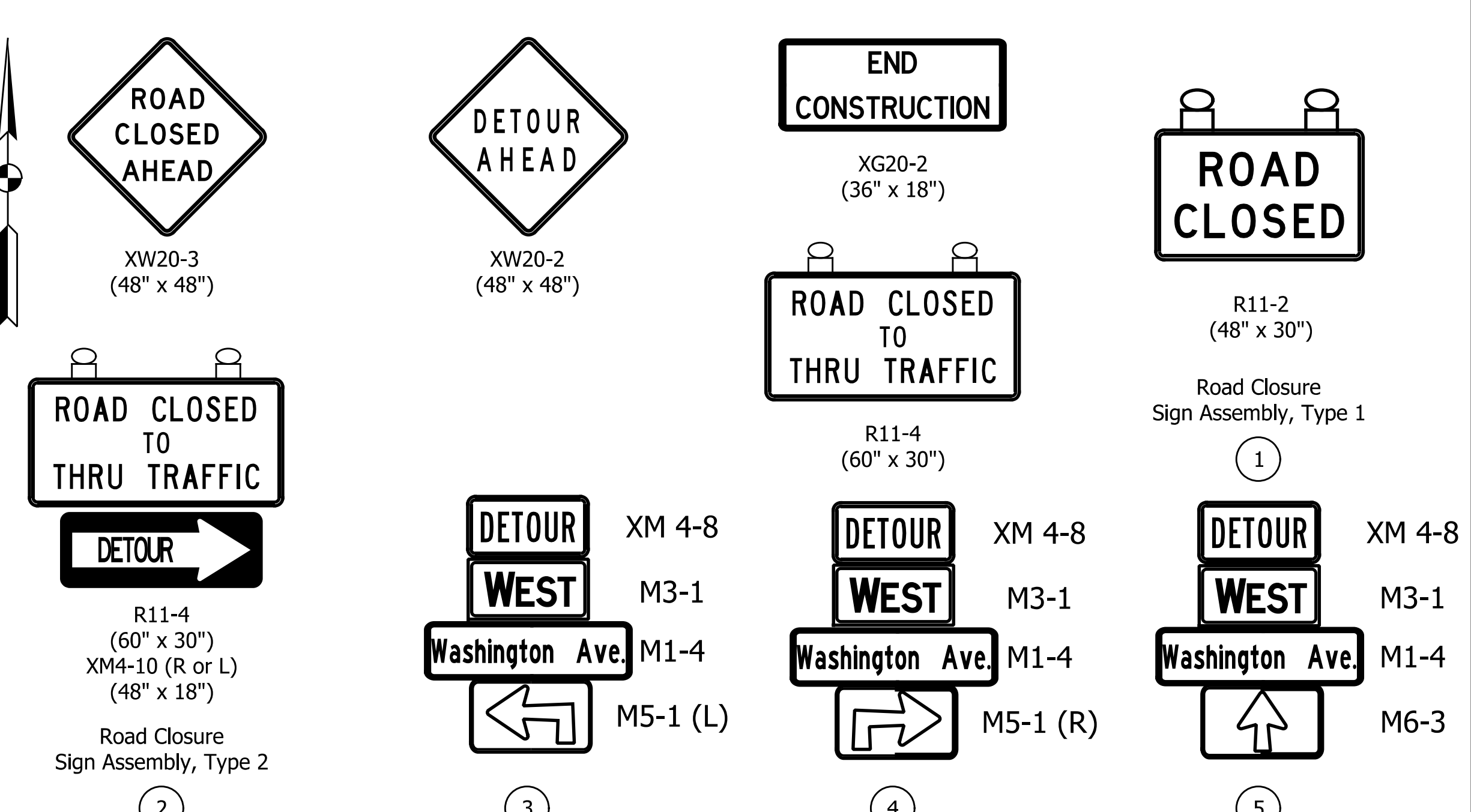


LEGEND

- (K) 165 #/Sys. QC/QA-HMA, 3, 64, Surface, 9.5 mm on 275 #/Sys. QC/QA-HMA, 3, 64, Intermediate, 19.0 mm on 385 #/Sys. QC/QA-HMA, 3, 64, Base, 19.0 mm on 330 #/Sys. QC/QA-HMA, 3, 64, Base, 19.0 mm on Subgrade Treatment, Type IBC
- (F) Sidewalk, Concrete, 4 in.
- (17) Combined Concrete Curb and Gutter
- (27) Sodding

NOTE TO REVIEWER:
Pavement design is in preliminary stage.
Final design to be determined at a later submittal

DATE	REVISION																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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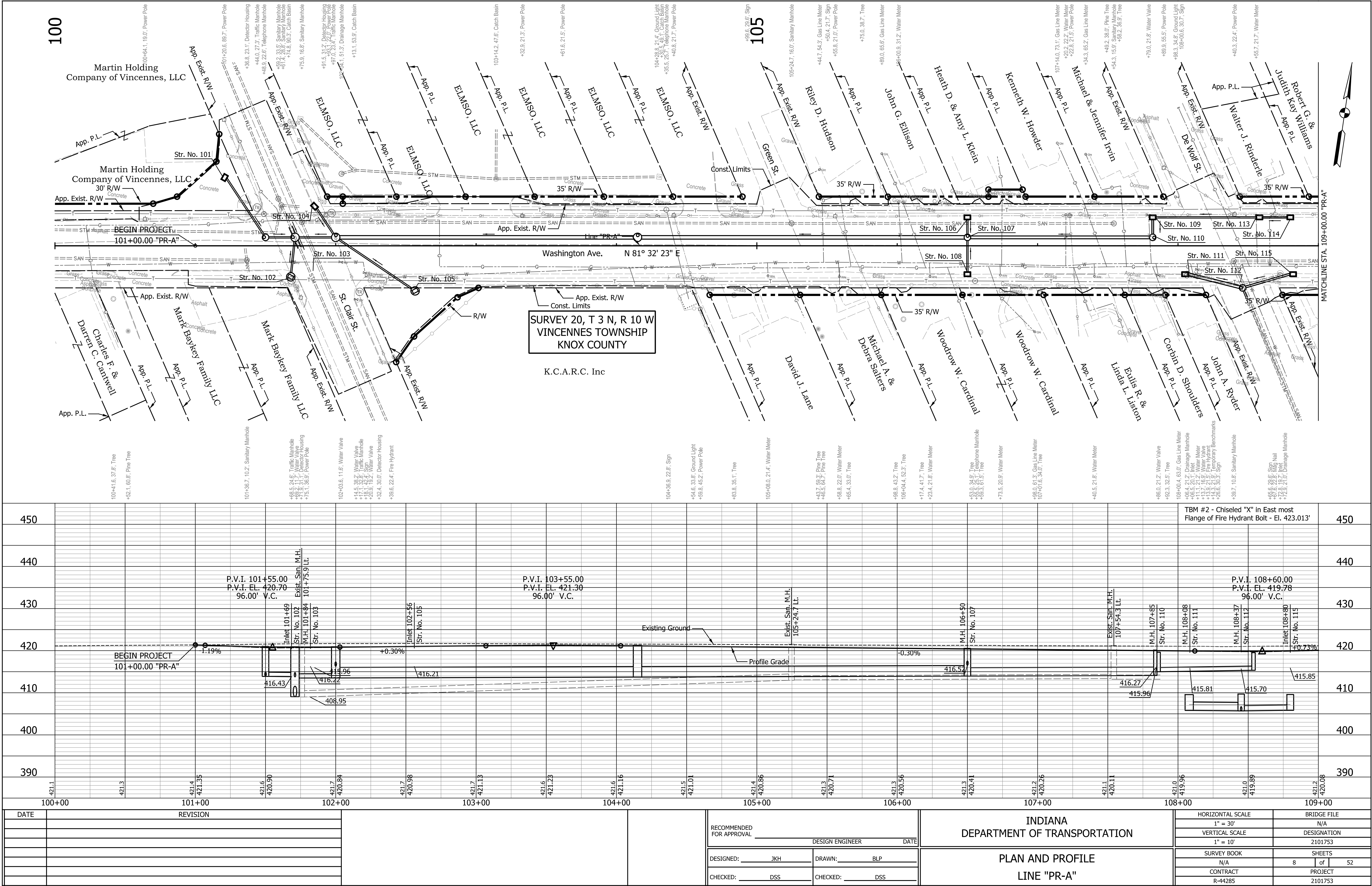
Note:
The Contractor Shall Maintain Access to Businesses
and Residents During Construction.

Thru pedestrians on north side of Washington Ave. will be routed down Indiana Ave. via St. Clair St. and State St. A short term closure (7 calendar days) of the north side sidewalk on Washington Ave. from State St. to Emison Ave. will be permitted to reconstruct this segment of sidewalk in order to use it for pedestrian MOT.

Thru pedestrians on south side of Washington Ave. will be routed down 11th St. via St. Clair St. and State St. A short term closure (7 calendar days) of the south side sidewalk on Washington Ave. from State St. to Emison Ave. will be permitted to reconstruct this segment of sidewalk in order to use it for pedestrian MOT.

DATE	REVISION	<div><div><div></div></div><div><div></div></div><div><div></div></div><div><div></div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> <div><div></div></div> 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Ind-Std-616c
Indiana Standard



100

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430

420

410

400

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450

440

430

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390

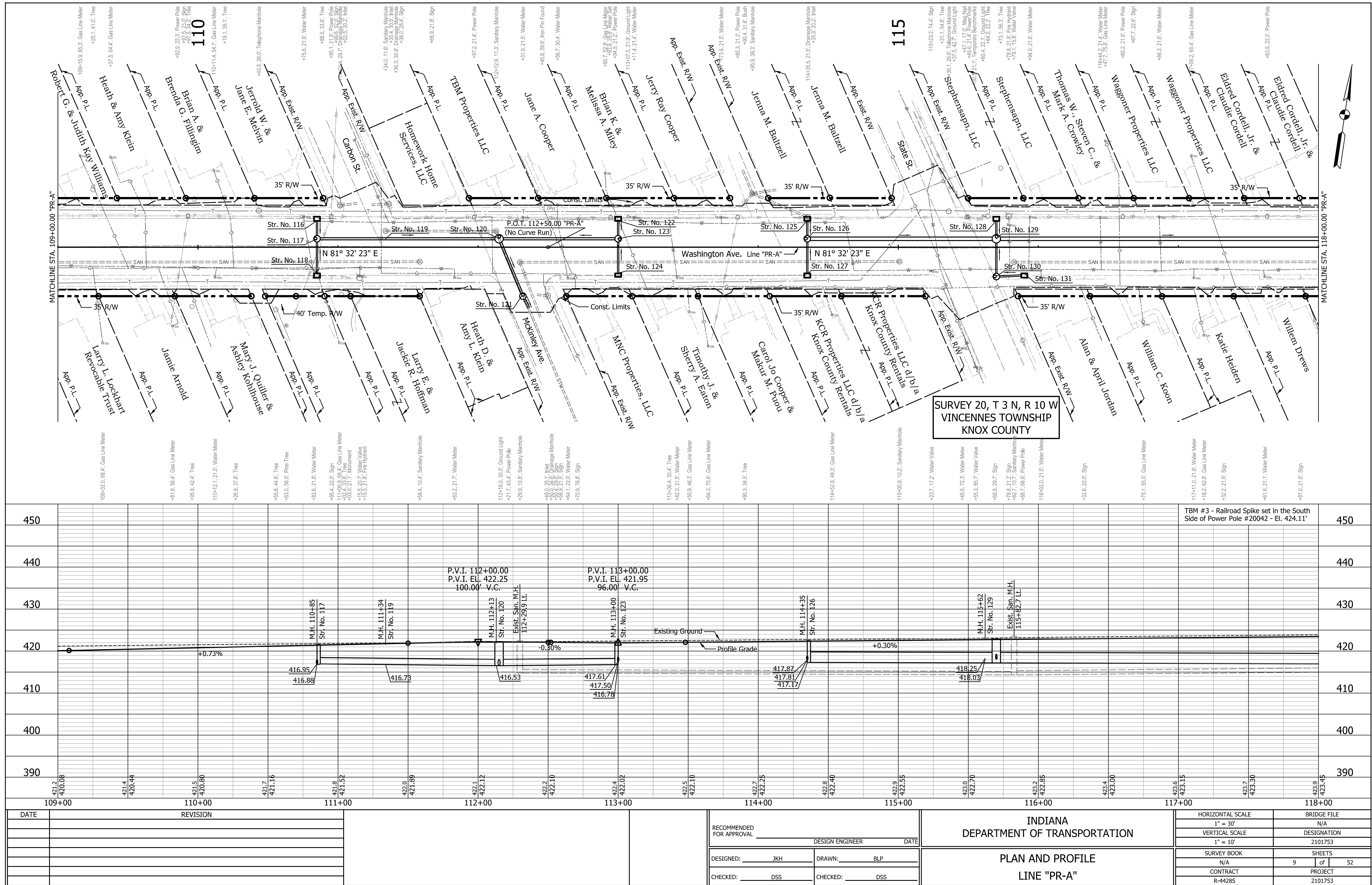
DATE	REVISION

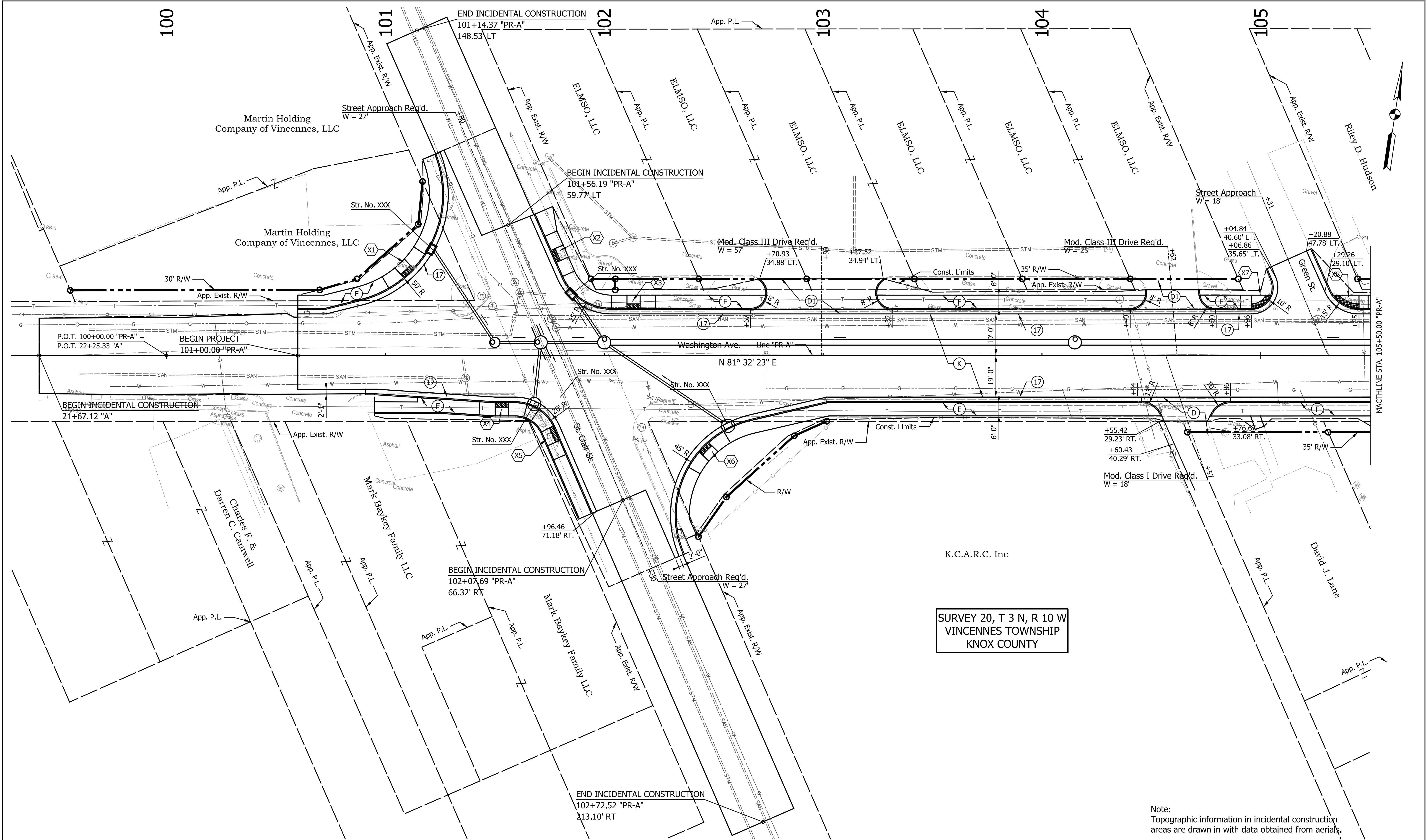
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: JKH		DRAWN: BLP			
CHECKED: DSS		CHECKED: DSS			

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
LINE "PR-A"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 30'		N/A	
VERTICAL SCALE		DESIGNATION	
1" = 10'		2101753	
SURVEY BOOK		SHEETS	
N/A		8 of 52	
CONTRACT		PROJECT	
R-44285		2101753	





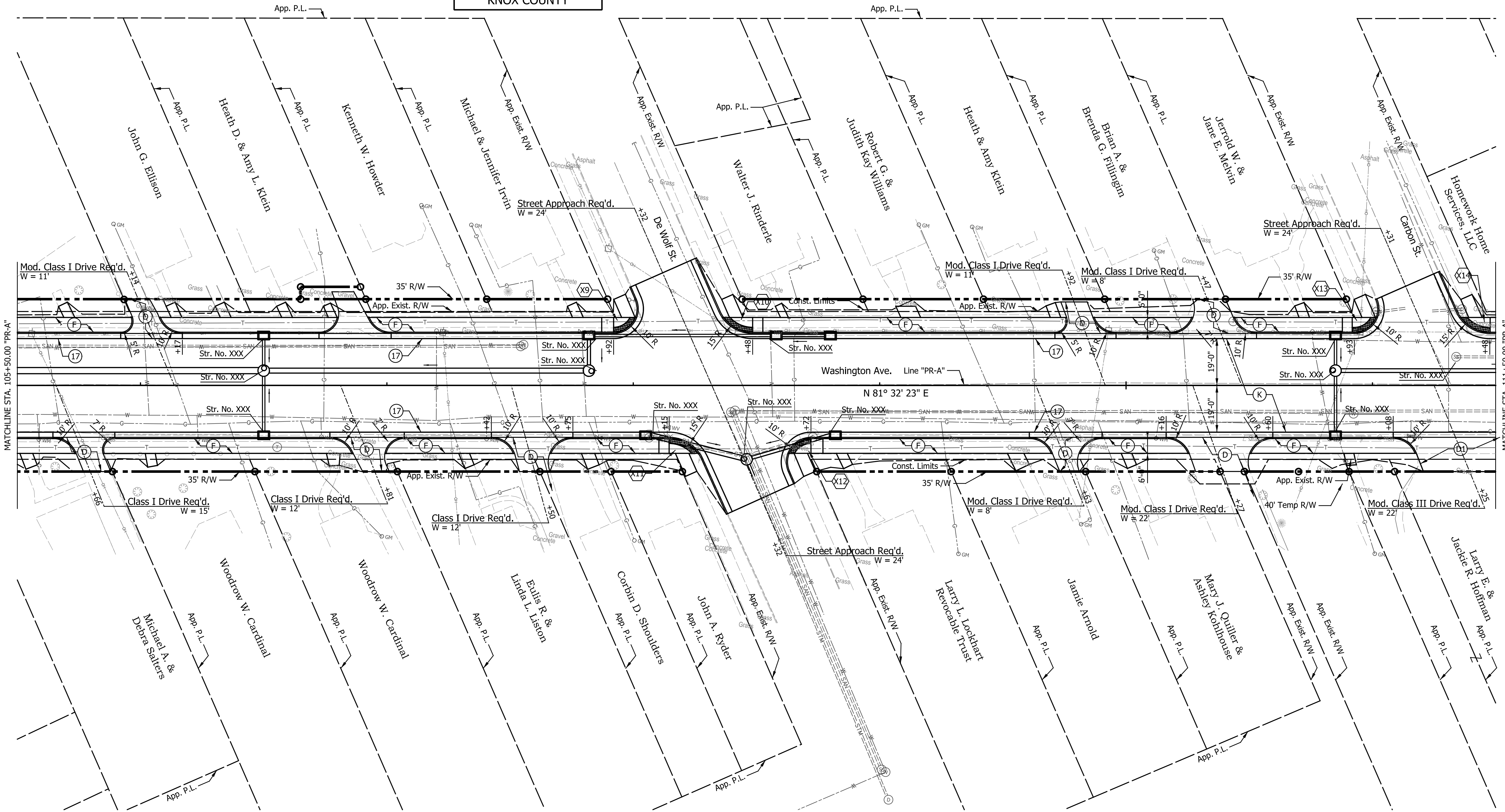
SURVEY 20, T 3 N, R 10 W
VINCENNES TOWNSHIP
KNOX COUNTY

Note:
Topographic information in incidental construction
areas are drawn in with data obtained from aerials.

DATE	REVISION	LEGEND		RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
		(D) PCCP for Approaches, 6 in., on Dense Graded Subbase, 6 in., on Subgrade Treatment Type II (6 in. Coarse Aggregate No. 53)	(K) Full Depth HMA Pavement (See Typical Sections)			CONSTRUCTION DETAILS		1" = 20'	N/A
		(D1) PCCP for Approaches, 9 in., on Dense Graded Subbase, 6 in., on Geogrid Type 1B, on Subgrade Treatment Type II (6 in. Coarse Aggregate No. 53)	(17) Combined Concrete Curb and Gutter	DESIGNED: JKH	DRAWN: BLP			VERTICAL SCALE	DESIGNATION
		(F) Sidewalk (See Typical Sections)	(X) Curb Ramp (See Spot Elevations)	CHECKED: DSS	CHECKED: DSS			N/A	2101753
								SURVEY BOOK	SHEETS
								N/A	11 of 52
								CONTRACT	PROJECT
								R-44285	2101753

Ind-001-01-01-01
Indiana Statewide

SURVEY 20, T 3 N, R 10 W
VINCENNES TOWNSHIP
KNOX COUNTY



DATE	REVISION

- D

PCCP for Approaches, 6 in., on Dense Graded Subbase, 6 in., on Subgrade Treatment Type II (6 in. Coarse Aggregate No. 53)

D1

PCCP for Approaches, 9 in., on Dense Graded Subbase, 6 in., on Geogrid Type IB, on Subgrade Treatment Type II (6 in. Coarse Aggregate No. 53)

F

Sidewalk (See Typical Sections)
- LEGEND

K

Full Depth HMA Pavement (See Typical Sections)

17

Combined Concrete Curb and Gutter

X

Curb Ramp (See Spot Elevations)

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JKH	DRAWN: BLP	
CHECKED: DSS	CHECKED: DSS	

INDIANA DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DETAILS

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 2101753
SURVEY BOOK N/A	SHEETS 12 of 52
CONTRACT R-44285	PROJECT 2101753

Indo-Ad/24/26/27
Indiana_Shae/20/21

DATE	REVISION

①	PCCP for Approaches, 6 in., on Dense Graded Subbase, 6 in., on Subgrade Treatment Type II (6 in. Coarse Aggregate No. 53)	LEGEND	Ⓚ	Full Depth HMA Pavement (See Typical Sections)
①1	PCCP for Approaches, 9 in., on Dense Graded Subbase, 6 in., on Geogrid Type 1B, on Subgrade Treatment Type II (6 in. Coarse Aggregate No. 53)	17	Combined Concrete Curb and Gutter	
Ⓕ	Sidewalk (See Typical Sections)	ⓧ	Curb Ramp (See Spot Elevations)	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JKH	DRAWN: BLP	
CHECKED: DSS	CHECKED: DSS	

INDIANA
DEPARTMENT OF TRANSPORTATION

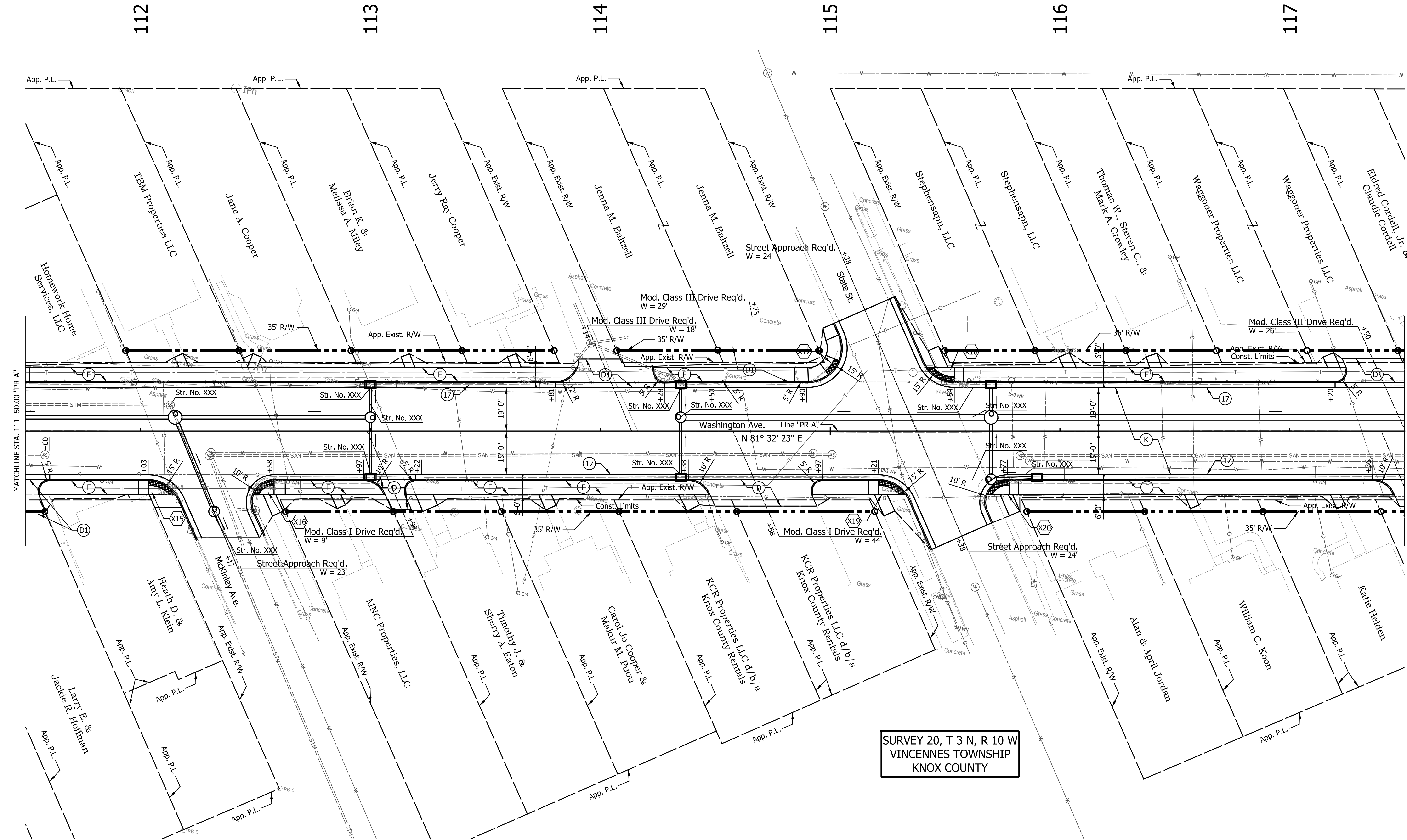
CONSTRUCTION DETAILS

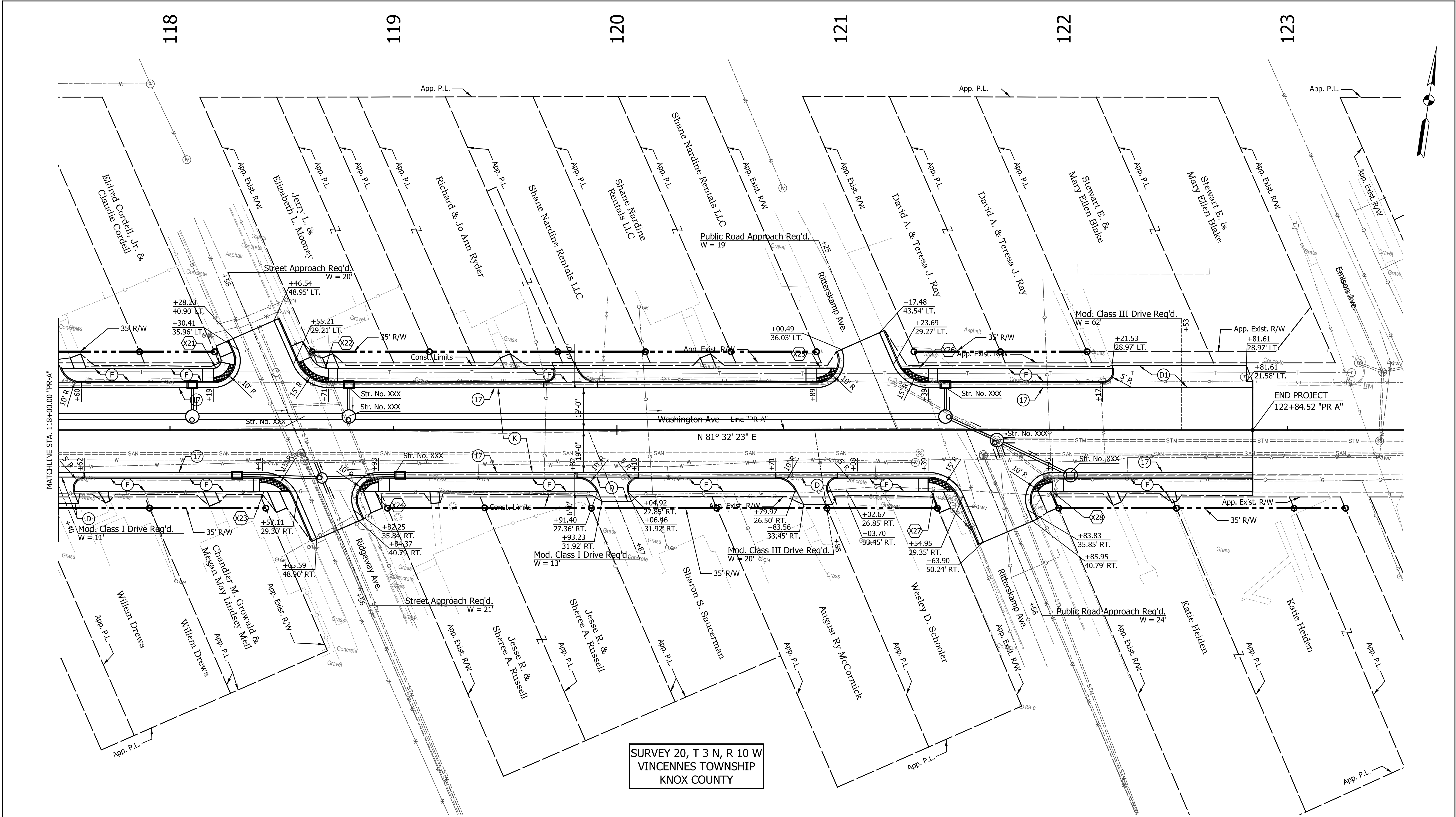
HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	2101753
SURVEY BOOK	SHEETS
N/A	13 of 52
CONTRACT	PROJECT
R-44285	2101753

MATCHLINE STA. 111+50.00 "PR-A"

MATCHLINE STA. 117+50.00 "PR-A"

SURVEY 20, T 3 N, R 10 W
VINCENNES TOWNSHIP
KNOX COUNTY





SURVEY 20, T 3 N, R 10 W
VINCENNES TOWNSHIP
KNOX COUNTY

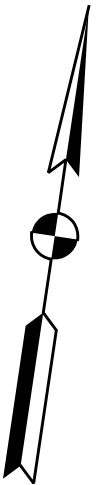
DATE	REVISION

①	PCCP for Approaches, 6 in., on Dense Graded Subbase, 6 in., on Subgrade Treatment Type II (6 in. Coarse Aggregate No. 53)	LEGEND	Ⓚ	Full Depth HMA Pavement (See Typical Sections)
①1	PCCP for Approaches, 9 in., on Dense Graded Subbase, 6 in., on Geogrid Type IB, on Subgrade Treatment Type II (6 in. Coarse Aggregate No. 53)	17	Combined Concrete Curb and Gutter	
ⓕ	Sidewalk (See Typical Sections)	ⓧ	Curb Ramp (See Spot Elevations)	

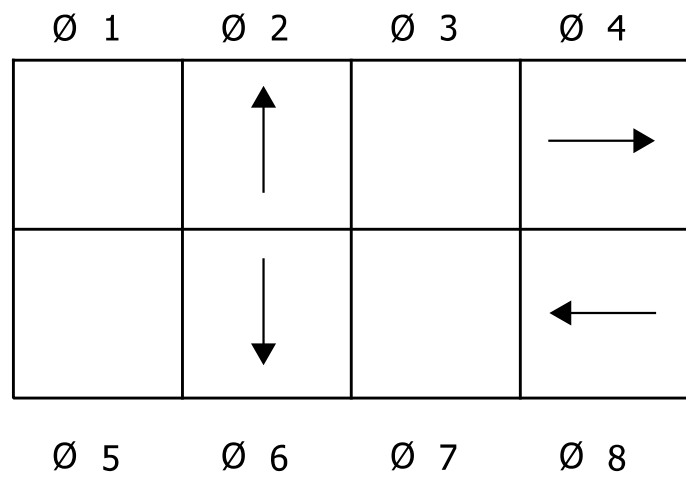
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JKH	DRAWN: BLP	
CHECKED: DSS	CHECKED: DSS	

INDIANA DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DETAILS

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 2101753
SURVEY BOOK N/A	SHEETS 14 of 52
CONTRACT R-44285	PROJECT 2101753



PHASE DIAGRAM



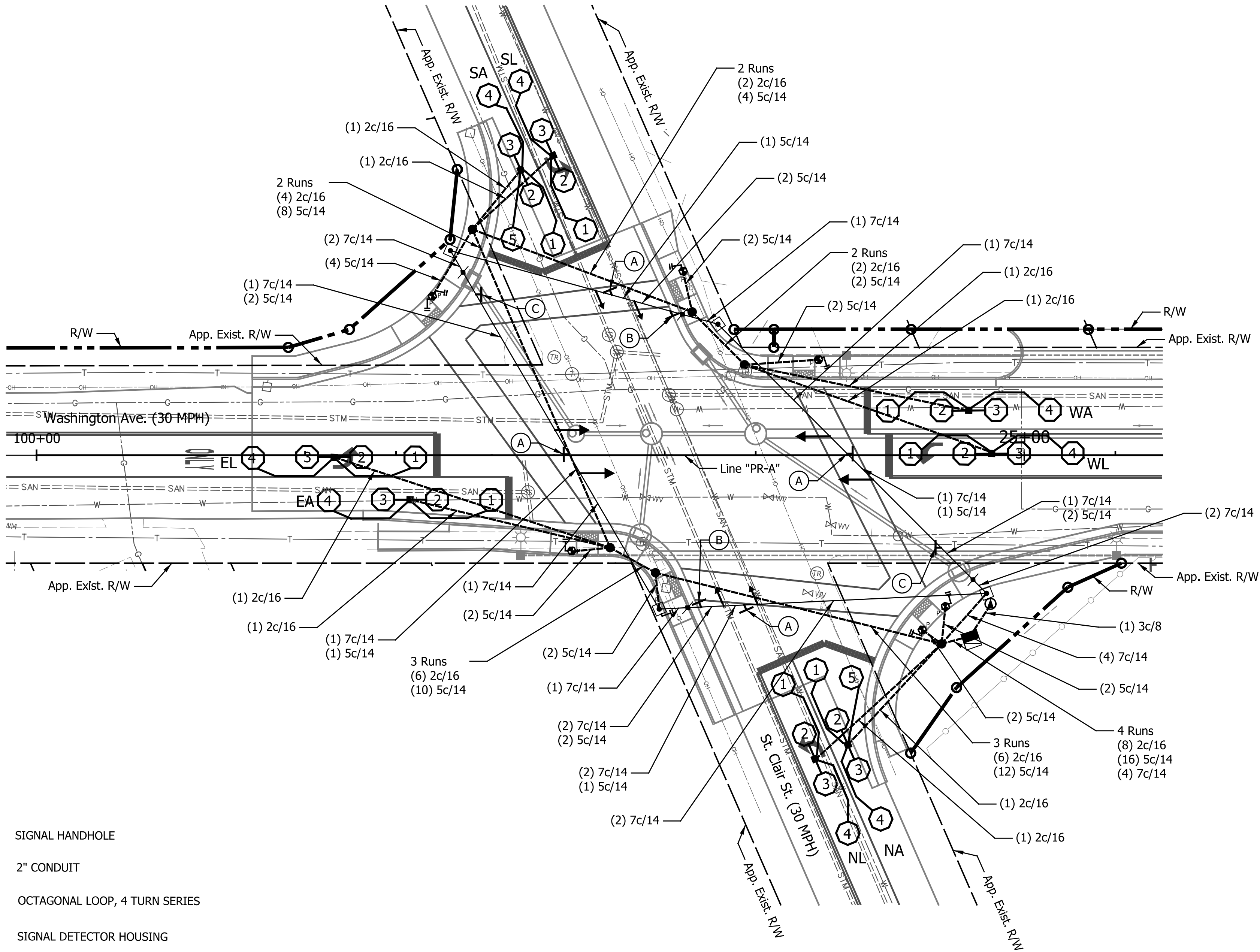
ALL APPROACHES FLASH RED

LOOP TAGGING TABLE

LANE	TAG NUMBER
NA	NA 2 - 1,2,3,4,5
NL	NL 2 - 1,2,3,4
EA	EA 4 - 1,2,3,4
EL	EL 4 - 1,2,3,4
SA	SA 6 - 1,2,3,4,5
SL	SL 6 - 1,2,3,4
WA	WA 8 - 1,2,3,4
WL	WL 8 - 1,2,3,4

SIGNAL LEGEND

- CONTROLLER AND "P-1" CABINET
ON "P-1" FOUNDATION
- 36" STEEL STRAIN POLE & FOUNDATION
- TRAFFIC SIGNAL HEAD, 3 FACE, 12" :
RED, AMBER, GREEN
- SIGNAL PEDESTAL POLE, 10', ON "A" FOUNDATION
- ACCESSIBLE PEDESTRIAN PUSH BUTTON & SIGN
- COUNTDOWN PEDESTRIAN SIGNAL INDICATION, 18"
- SIGNAL HANDHOLE
- 2" CONDUIT
- OCTAGONAL LOOP, 4 TURN SERIES
- SIGNAL DETECTOR HOUSING
- TRAFFIC SIGN
- SIGNAL SERVICE METER, POLE MOUNTED



SIGN LEGEND



R3-5(L)
30" x 36"
(A)

Washington Ave

D3-1
24" x 120"
(B)

St. Clair St

D3-1
24" x 96"
(C)

NOTES

1.

PAVEMENT MARKINGS ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY. SEE THE SIGNING AND PAVEMENT MARKING PLANS FOR CONSTRUCTION.
2.

ALL EXISTING SIGNAL EQUIPMENT TO BE REMOVED.
3.

AUDIBLE PUSH BUTTONS SHALL BE LOCATED PERPENDICULAR TO THE PEDESTRIAN CROSSING AND SHALL HAVE A SIDE REACH OF 10" MAXIMUM.

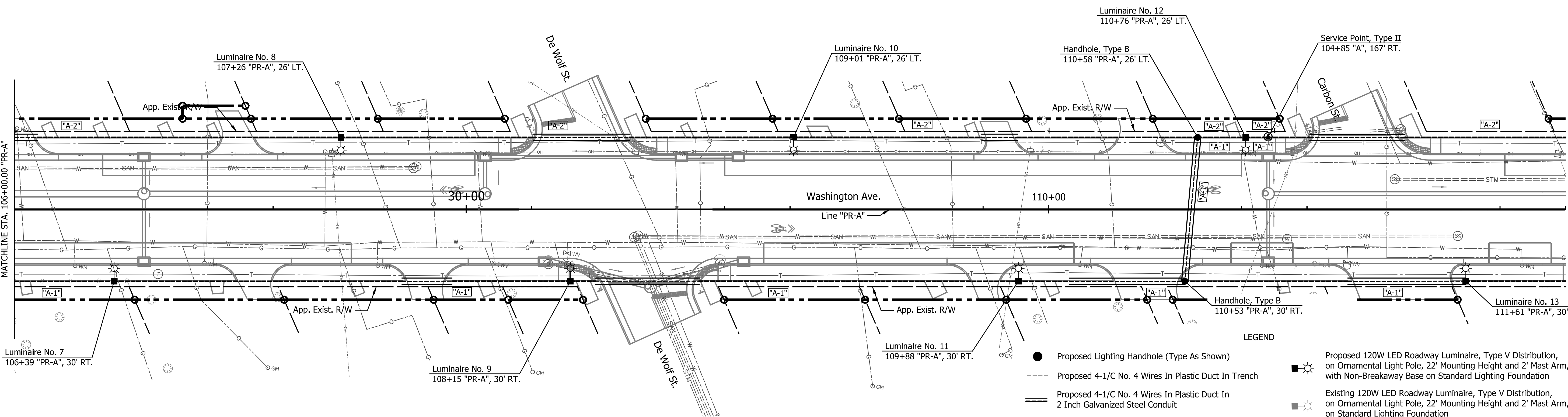
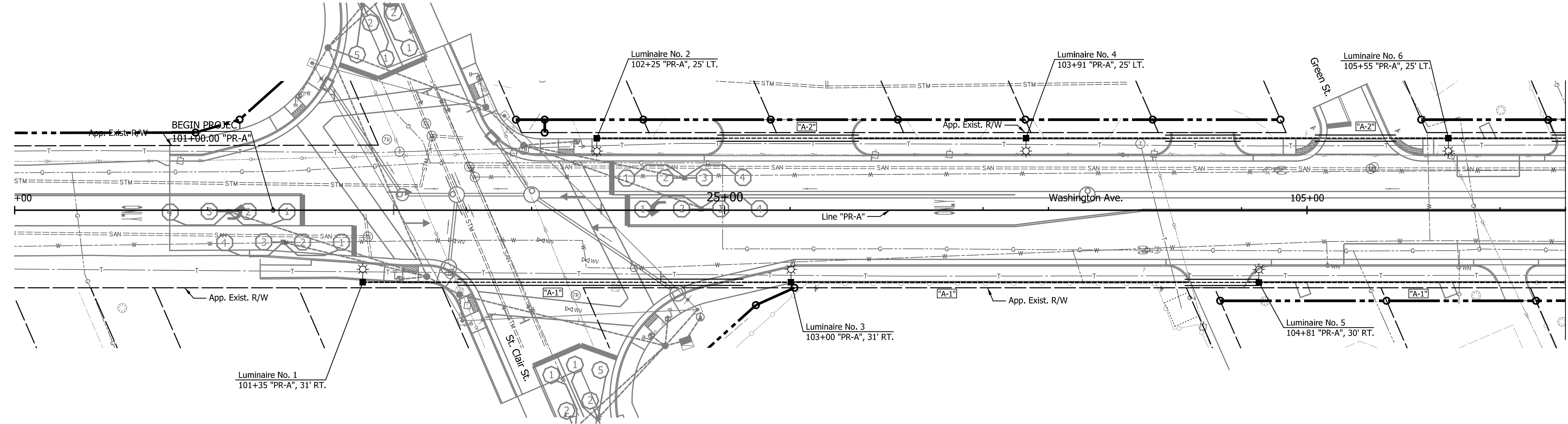
DATE	REVISION

RECOMMENDED FOR APPROVAL _____	
DESIGN ENGINEER	DATE
DESIGNED: _____ ASC	DRAWN: _____ ASC
CHECKED: _____ GMG	CHECKED: _____ GMG

INDIANA
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL DETAILS

HORIZONTAL SCALE	BRIDGE FILE	
1" = 20'	N/A	
VERTICAL SCALE	DESIGNATION	
N/A	2101753	
SURVEY BOOK	SHEETS	
N/A	15	of 52
CONTRACT	PROJECT	
R-44285	2101753	



LEGEND

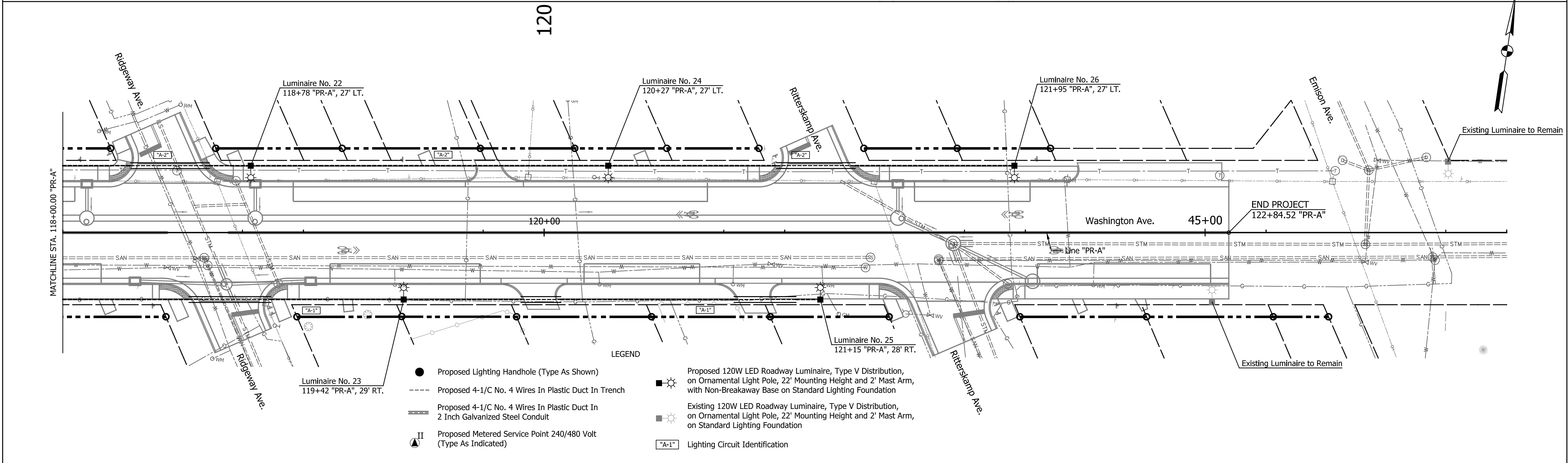
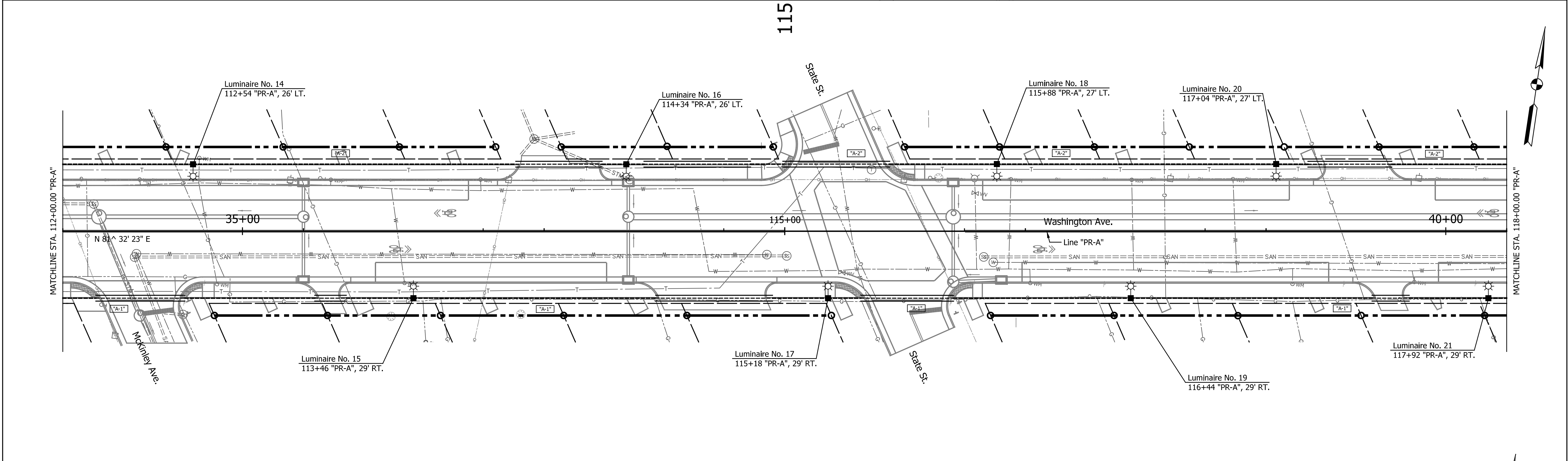
- Proposed Lighting Handhole (Type As Shown)
- Proposed 4-1/C No. 4 Wires In Plastic Duct In Trench
- === Proposed 4-1/C No. 4 Wires In Plastic Duct In 2 Inch Galvanized Steel Conduit
- II ▲ Proposed Metered Service Point 240/480 Volt (Type As Indicated)
- ☀ Proposed 120W LED Roadway Luminaire, Type V Distribution, on Ornamental Light Pole, 22' Mounting Height and 2' Mast Arm, with Non-Breakaway Base on Standard Lighting Foundation
- ☀ Existing 120W LED Roadway Luminaire, Type V Distribution, on Ornamental Light Pole, 22' Mounting Height and 2' Mast Arm, on Standard Lighting Foundation
- "A-1" Lighting Circuit Identification

DATE	REVISION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: GWK	DRAWN: GWK	
CHECKED: GMG	CHECKED: GMG	

INDIANA DEPARTMENT OF TRANSPORTATION
LIGHTING DETAILS

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 2101753
SURVEY BOOK N/A	SHEETS 16 of 52
CONTRACT R-44285	PROJECT 2101753



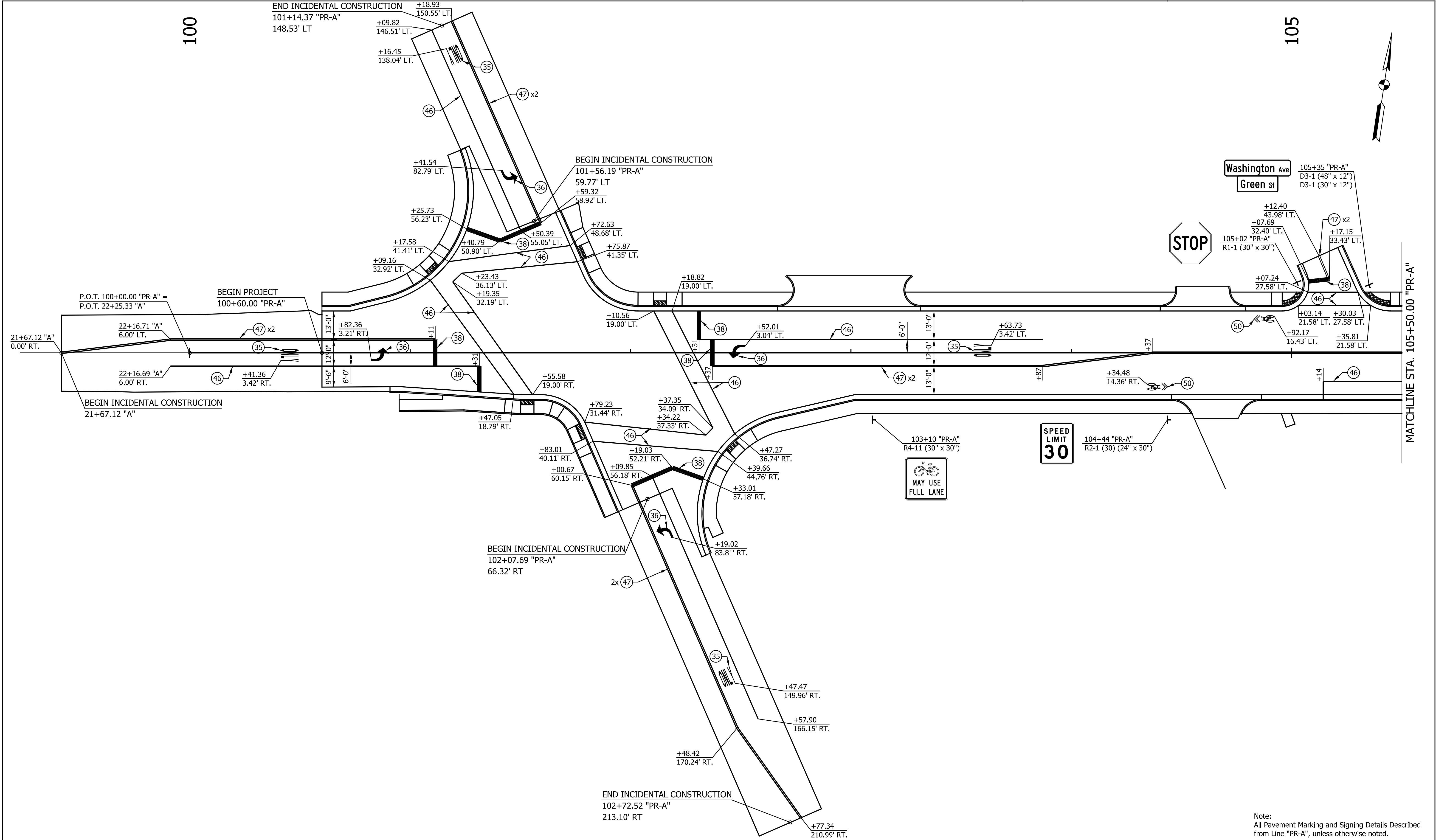
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										1" = 20'			N/A
										VERTICAL SCALE			DESIGNATION
										N/A			2101753
										SURVEY BOOK			SHEETS
										N/A		17	of 52
										CONTRACT			PROJECT
										R-44285			2101753

DESIGNED: GWK	DRAWN: GWK
CHECKED: GMG	CHECKED: GMG

DATE	DESIGN ENGINEER

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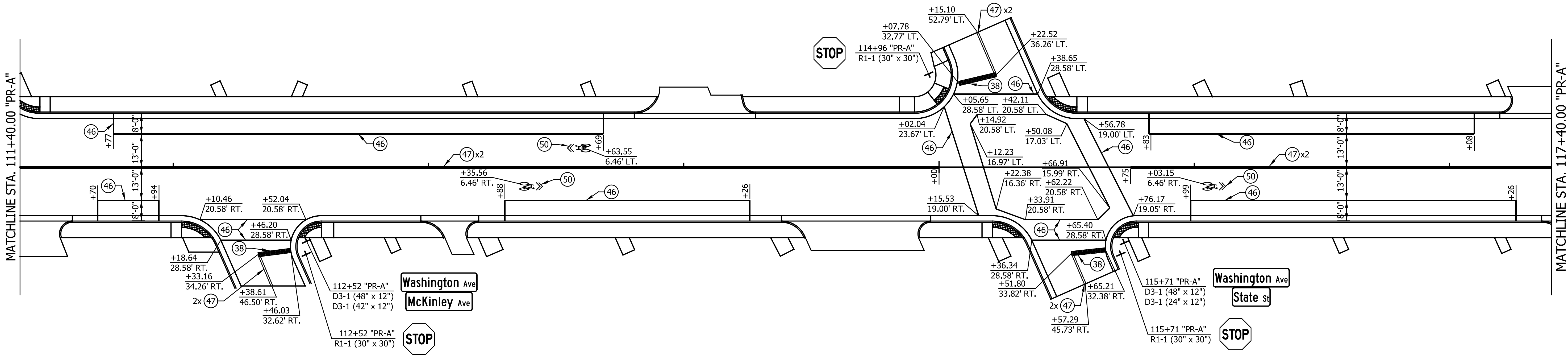
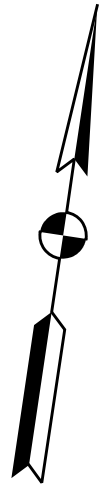
Ind-Std-016-0
Indiana_Sheet.tbl



DATE	REVISION	LEGEND		RECOMMENDED FOR APPROVAL	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE		
		(35)	Pavement Message Marking, Thermoplastic, ONLY, White				1" = 20'		N/A		
		(36)	Pavement Message Marking, Thermoplastic, Lane Indication Arrow, White	DESIGN ENGINEER		VERTICAL SCALE		DESIGNATION			
		(38)	Transverse Marking, Thermoplastic, Stop Line, White, 24 in.	DATE		N/A		2101753			
		(46)	Line, Thermoplastic, Solid, White, 6 in.	DESIGNED: JKH	DRAWN: BLP	PAVEMENT MARKING AND SIGNING DETAILS		SURVEY BOOK		SHEETS	
		(47)	Line, Thermoplastic, Solid, Yellow, 6 in.					N/A		18 of 52	
		(50)	Pavement Message Marking, Thermoplastic, Bike w/ Arrow	CHECKED: DSS	CHECKED: DSS	CONTRACT		PROJECT			
						R-44285		2101753			



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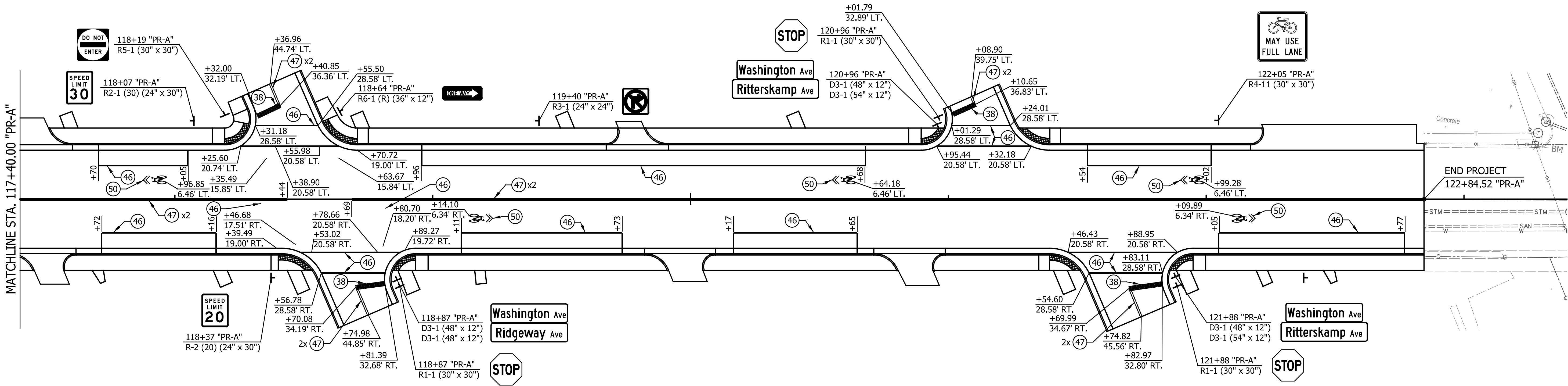
DATE	REVISION

LEGEND	
(35)	Pavement Message Marking, Thermoplastic, ONLY, White
(36)	Pavement Message Marking, Thermoplastic, Lane Indication Arrow, White
(38)	Transverse Marking, Thermoplastic, Stop Line, White, 24 In.
(46)	Line, Thermoplastic, Solid, White, 6 in.
(47)	Line, Thermoplastic, Solid, Yellow, 6 in.
(50)	Pavement Message Marking, Thermoplastic, Bike w/ Arrow

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: JKH	DRAWN: BLP
CHECKED: DSS	CHECKED: DSS

INDIANA DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING AND SIGNING DETAILS	

HORIZONTAL SCALE	BRIDGE FILE	
1" = 20'	N/A	
VERTICAL SCALE	DESIGNATION	
N/A	2101753	
SURVEY BOOK	SHEETS	
N/A	20	of 52
CONTRACT	PROJECT	
R-44285	2101753	



DATE	REVISION	LEGEND		RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
							1" = 20'		N/A	
		(35) Pavement Message Marking, Thermoplastic, ONLY, White			PAVEMENT MARKING AND SIGNING DETAILS		VERTICAL SCALE		DESIGNATION	
		(36) Pavement Message Marking, Thermoplastic, Lane Indication Arrow, White					N/A		2101753	
		(38) Transverse Marking, Thermoplastic, Stop Line, White, 24 in.			DESIGNED: _____ JKH _____ DRAWN: _____ BLP _____		SURVEY BOOK		SHEETS	
		(46) Line, Thermoplastic, Solid, White, 6 in. (47) Line, Thermoplastic, Solid, Yellow, 6 in.					N/A		21 of 52	
		(50) Pavement Message Marking, Thermoplastic, Bike w/ Arrow			CHECKED: _____ DSS _____ CHECKED: _____ DSS _____		CONTRACT		PROJECT	
							R-44285		2101753	

Appendix C

Early Coordination



Sample Early
Coordination Letter

Certified MBE, State of Indiana; City of Indianapolis

INDOT Certified DBE

July 26, 2023

RE: Des. No. 2101753, City of Vincennes – Washington Avenue Phase II, from Emison Street to East Clair Street, Road Reconstruction Project, Knox County, Indiana.

Environmental Reviewer,

The City of Vincennes, with partial funding from the Federal Highway Administration (FHWA) intend to proceed with the reconstruction of Washington Avenue (Ave) from East (E) Clair Street (St) to Emison St, in the City of Vincennes, Knox County, Indiana. This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible effects associated with this project. Please use the above designation number and description in your reply. We will incorporate your comments into a study of the project's environmental impacts.

This project is located on Washington Ave, Township 3 North, Range 10 West, Vincennes Township, Knox County. This section of Washington Ave is a two-lane rural principal arterial. The existing road at the project area consists of two lanes, approximately 16 feet (ft) wide, with 5-ft sidewalks. There is a third turning lane at the intersection Washington Ave and E Clair St, turning into E Clair St Northbound (NB).

The proposed project will include the replacement and rehabilitation of the pavement on Washington Ave from E Clair St to Emison St, and will consist of milling, widening, and overlay. The curb-to-curb width of the existing roadway will be widened from 40 feet(ft) to 42 ft. The existing sidewalks on both sides of Washington Ave will be widened from 5 ft to 6 ft. Additional improvements will include gutters, sidewalks, Americans with Disabilities Act (ADA) compliant curb ramps, street lighting throughout, and an updated traffic signal at the Washington Ave and St. Clair St intersection. This project will require the complete excavation and removal of the former United States (US) Highway 50 roadbed and trolley track remnants. Maintenance of Traffic (MOT) for this project is anticipated to be a complete closure of Washington Ave within the project area. A detour route is anticipated to utilize Niblack Boulevard (Blvd), North (N) 6th St, College Ave, and add 1.52 miles of additional travel distance. It is anticipated that 0.03 acres of permanent right-of-way (ROW) and 0.90 acres of temporary ROW acquisition will be required for this project. Some tree removal is anticipated. Construction is anticipated to begin fall of 2026.

Land use in the vicinity of the project is primarily residential with commercial facilities on the north and south sides of Washington Ave. The project qualifies for the application of the USFWS range wide programmatic informal consultation for the Indiana Bat and Northern Long-Eared Bat and project information will be provided to the USFWS for review separately. SJCA Inc. will investigate the site for archeological and historic resources for compliance with Section 106 and send relevant findings to the State Historic Preservation Office (SHPO).

Please provide your response within thirty (30) calendar days from the date of this letter. However, should you find that an extension to the response time is necessary, a reasonable amount of time may be granted upon request. If you have any questions regarding this matter, please feel free to contact Taylor Gabrysiak, SJCA Inc., tgabrysiak@sjcainc.com, or John Sprague, Employee in Responsible Charge, jsprague@vincennes.org. Thank you in advance for your input.

Sincerely,

Taylor Gabrysiak
GIS Specialist
SJCA Inc.

Attachments:

Early Coordination Recipient List
Project Maps (Location, Topographic, Aerial)
Photo Location Map and Site Photographs

Maps and Photographs are available in Appendix B



The following agencies received Early Coordination Letters:

Federal Highway Administration
Vincennes District, Patrick Carpenter
patrick.carpenter@dot.gov

IDEM Groundwater Section
Section Chief, Alisha Turnbow
ATurnbow@idem.in.gov

National Park Service
Midwest Regional Office
Regional Environmental Coordinator
Mwro_Compliance@nps.gov

Indiana Geological and Water Survey
<https://igws.indiana.edu/eAssessment>

IDNR Environmental Coordinator
environmentalreview@dnr.in.gov

US Department of Housing and Urban Development
Chicago Regional Office
Erik.r.sandstedt@hud.gov

INDOT Vincennes District
Environmental Section Manager, Ryan Falls
RFalls@indot.in.gov

INDOT Project Manager, Aubrey Howder
ahowder@indot.in.gov

U.S. Fish and Wildlife Service
Bloomington Indiana Field Office
robin_mcowilliams@fws.gov

Natural Resources Conservation Service
John.allen@usda.gov

Knox County Commissioners
President, Kellie Streeter
kellie.streeter@knoxcounty.in.gov

Knox County Highway Department
Engineer/Superintendent, Benji Boyd
bboyd@knoxcounty.in.gov

Knox County Surveyor, Richard Vermillion
surveyor@knoxcounty.in.gov

Vincennes Community School Corporation (VCSC)
Superintendent, Greg Parsley
parsleyg@vcsc.k12.in.us

City of Vincennes Parks and Recreation
Superintendent, Chris Moore
cmoore@vincennes.in.gov

City of Vincennes Mayor Office
Administrative Assistant, Cheryl Hacker
chacker@vincennes.in.gov

Washington Elementary School
Principal and VCSC Transportation Director, Jared Blue
bluej@vcsc.k12.in.us
hiddet@vcsc.k12.in.us

Knox County Emergency Management
Director, John Streeter
ema@knoxcounty.in.gov

Vincennes Fire Department
bbobe@vincennes.in.gov

Vincennes Police Department
Aaron Luce, Information Officer
aluce@vincennes.in.gov

also sent to
Civitan Childrens Center
info@knoxcountyarc.com- 8.3.23
Vincennes MS4 Coordinator
jakep@vinutilities.com- 8.24.23

Organization and Project Information

Project ID:

Des. ID: 2101753

Project Title: City of Vincennes Washington Ave. Phase II in the Vincennes District

Name of Organization: SJCA Inc

Requested by: Taylor Gabrysiak

Environmental Assessment Report

1. Geological Hazards:

- Potential Mine Subsidence ([CMIS](#))
- High liquefaction potential
- 1% Annual Chance Flood Hazard

2. Mineral Resources:

- Bedrock Resource: Low Potential
- Sand and Gravel Resource: High Potential

3. Active or abandoned mineral resources extraction sites:

- Underground Coal Mines

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

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This information was furnished by Indiana Geological Survey

Address: 1001 E. 10th St., Bloomington, IN 47405

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: June 02, 2023

August 1, 2023

Taylor Gabrysiak
SJCA
9102 N Meridian Street, Suite 200
Indianapolis, Indiana 46260

Dear Taylor Gabrysiak:

The proposed roadway reconstruction project on Washington Avenue Phase II, from Emison Street to East Clair Street in Knox County, Indiana (Des. No. 2101753) as referred to in your letter received July 26, 2023, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859 or john.allen@usda.gov.

Sincerely,

JOHN ALLEN Digitally signed by JOHN ALLEN
Date: 2023.08.03 05:41:05 -04'00'

JOHN ALLEN
State Soil Scientist

Enclosers



INDIANA DEPARTMENT OF ENVIRONMENTAL MANAGEMENT

We Protect Hoosiers and Our Environment.

100 N. Senate Avenue • Indianapolis, IN 46204
(800) 451-6027 • (317) 232-8603 • www.idem.IN.gov

Eric J. Holcomb
Governor

Brian C. Rockensuess
Commissioner

August 4, 2023

SJCA Inc.
Attention: Taylor Gabrysiak
9102 North Meridian Street, Suite 200
Indianapolis, IN 46260

Dear Taylor Gabrysiak:

Re: Wellhead Protection Area
Proximity Determination
Des No 2101753
City of Vincennes – Washington Avenue Phase II,
from Emison Street to East Clair Street,
Road Reconstruction
Project, Knox County, Indiana

Upon review of the above referenced project site, it has been determined that the proposed project area **is not located within** a Wellhead Protection Area. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases, we use a 3,000-foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/idem/cleanwater/2456.htm> and scroll to the bottom of the page.

The project area **is not located within** a Source Water Assessment Area for a PWSS's surface water intake. The Source Water Assessment Area relates to the surface water drainage area that water could potentially flow and influence water quality for a PWSS's source of drinking water.

In the future, **please consider using this self-service tool** if it suits your needs. The Drinking Water Branch has a self-service tool which allows one to determine wellhead proximity without submitting the application form. Go to <https://www.in.gov/idem/cleanwater/pages/wellhead/> and use the instructions at the bottom of the page.



Please Reduce, Reuse, Recycle

Taylor Gabrysiak
Page 2

If you have any additional questions, please feel free to contact me at the address above or at 317-233-9158 and aturnbow@idem.in.gov.

Sincerely,

A handwritten signature in black ink that reads "Alisha Turnbow". The signature is written in a cursive, flowing style.

Alisha Turnbow,
Environmental Manager
Ground Water Section
Drinking Water Branch
Office of Water Quality

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR#: ER-25814

Request Received: July 26, 2023

Requestor:

Taylor Gabrysiak
SJCA Inc
9102 North Meridian Street, Suite 200
Indianapolis, IN 46260

Project:

Washington Avenue Phase II road reconstruction and widening, from Emison Street to East Clair Street, City of Vincennes; Des #2101753.

County/Site Info: Knox County

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment:

Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database:

The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish and Wildlife Comments:

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

A) Riparian Habitat

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation Guidelines (and plant lists) can be found online at: <https://www.in.gov/nrc/files/IB-17.pdf>.

Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ratio based on area of impact. Impacts under 0.10 acre in an urban area should be mitigated by replacing trees that are 10" diameter-at-breast height (dbh) or greater by planting five trees, 1" to 2" in dbh, for each tree which is removed that is 10" dbh or greater. Seeding and stabilizing disturbed areas is required regardless of the impact amount and location. If floodway

impacts to forested wetland and non-wetland habitat areas combine to be 0.10 acres or more, mitigation should be done and coordinated with the biologist, as needed.

The mitigation site should be located in the floodway, downstream of the one (1) square mile drainage area of that stream (or another stream within the 8-digit HUC, preferably as close to the impact site as possible) and adjacent to existing forested riparian habitat.

B) Lighting

Certain types of LED lighting can have negative impacts on both human and wildlife health and safety. Scientific evidence suggests that artificial light at night has negative and deadly effects on many organisms including amphibians, birds, mammals, insects, and plants (<https://www.darksky.org/light-pollution/wildlife/>). A June 2016 American Medical Association (AMA) report, "Human and Environmental Effects of Light Emitting Diode Community Lighting," concluded that "white LED street lighting patterns may contribute to the risk of chronic disease in the populations of cities in which they have been installed."

The International Dark-Sky Association has developed a set of recommendations (<https://www.darksky.org/our-work/lighting/lighting-for-citizens/led-guide/>) for those choosing LED lighting systems. These suggestions will aid in the selection of lighting that is energy and cost efficient, yet ensures safety and security, protects wildlife, and promotes the goal of reducing light pollution:

- a. Always choose fully shielded fixtures that emit no light upward. The Division further recommends that light-shielding be used in forested areas to contain the light to the road corridor and avoid casting light beyond into forested habitat.
- b. Use "warm-white" or filtered LEDs (CCT < 3,000 K; S/P ratio < 1.2) to minimize harmful blue light emission.
- c. Look for products with adaptive controls like dimmers, timers, and motion sensors.
- d. Consider dimming or turning off lights during non-peak overnight hours.
- e. Avoid the temptation to over-light because of the higher luminous efficiency of LEDs.
- f. Only light the exact space and in the amount required for particular tasks.

The Division of Fish and Wildlife strongly encourages visiting the following link to learn more about the potential negative impacts of improperly selected LED lighting systems: <http://darksky.org/light-pollution/light-pollution-solutions/>.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers, as well as hardwood trees and shrubs if any woody plants are disturbed during construction, native to Southern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only. A native herbaceous seed mixture must include at least 5 species of grasses and sedges and 5 species of wildflowers.
2. Minimize and contain within the project limits all tree and brush clearing.
3. Do not cut any trees suitable for Indiana Bat or Northern Long-eared Bat roosting (3 inches or greater diameter-at-breast height, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the waterbody or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
5. If erosion control blankets are used, they shall be heavy-duty, biodegradable, and net free or use loose-woven/Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Our agency appreciates this opportunity to be of service. Please contact me at RVanVoorhis@dnr.IN.gov or (317) 232-8163 if we can be of further assistance.

Rachel Van Voorhis

Rachel Van Voorhis
Environmental Coordinator
Division of Fish and Wildlife

Date: August 25, 2023



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

August 09, 2023

Project code: 2023-0110753

Project Name: Des 2101753, City of Vincennes – Washington Avenue Phase II Road Reconstruction, Knox County

Subject: Concurrence verification letter for the 'Des 2101753, City of Vincennes – Washington Avenue Phase II Road Reconstruction, Knox County' project under the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (NLEB).

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated August 09, 2023 to verify that the **Des 2101753, City of Vincennes – Washington Avenue Phase II Road Reconstruction, Knox County** (Proposed Action) may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the endangered northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to section 7(a)(2) of ESA (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances,

Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities: If your initial bridge/culvert or structure assessment documented signs of bat use or occupancy, or an assessment failed to detect Indiana bats and/or NLEBs, yet are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of any potential take. In these instances, potential incidental take of Indiana bats and/or NLEBs is covered under the Incidental Take Statement in the 2018 FHWA, FRA, FTA PBO (provided that the take is reported to the Service).

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required.

For Proposed Actions that include bridge/culvert or structure removal, replacement, and/or maintenance activities:

If your initial bridge/culvert or structure assessments failed to detect Indiana bats and/or NLEB use or occupancy, yet bats are later detected prior to, or during construction, please submit the Post Assessment Discovery of Bats at Bridge/Culvert or Structure Form (User Guide Appendix E) to this Service Office within 2 working days of the incident. In these instances, potential incidental take of Indiana bats and/or NLEBs may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

The following species may occur in your project area and **are not** covered by this determination:

- Monarch Butterfly *Danaus plexippus* Candidate
- Tricolored Bat *Perimyotis subflavus* Proposed Endangered
- Whooping Crane *Grus americana* Experimental Population, Non-Essential

PROJECT DESCRIPTION

The following project name and description was collected in IPaC as part of the endangered species review process.

NAME

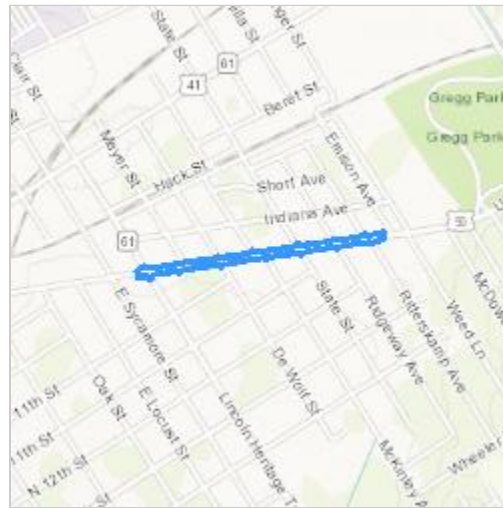
Des 2101753, City of Vincennes – Washington Avenue Phase II Road Reconstruction, Knox County

DESCRIPTION

This project is located on Washington Ave, from Emison Street to East Clair Street, in T-3-N, R-10-W, Vincennes Township, Knox County, Indiana. This road reconstruction project includes the replacement and rehabilitation of the pavement on Washington Ave from E Clair St to Emison St and will consist of milling, widening, and overlay. The curb-to-curb width of the existing roadway will be widened from 40 feet(ft) to 42 ft. The existing sidewalks on both sides of Washington Ave will be widened from 5 ft to 6 ft. Additional improvements will include gutters, sidewalks, Americans with Disabilities Act (ADA) compliant curb ramps, street lighting throughout, and an updated traffic signal at the Washington Ave and St. Clair St intersection. This project will require the complete excavation and removal of the former US Highway 50 roadbed and trolley track remnants.

Suitable summer habitat is not located within or adjacent to the project. Three (3) trees are anticipated to be removed for this project. All three (3) trees are unsuitable summer habitat because they are single, ornamental trees within an urban area; and there will be no federal time of year restrictions on removal. The species of trees to be removed are Red maple (*Acer rubrum*), Silver maple (*Acer saccharinum*), and Japanese cherry (*Prunus serrulata*). A review of the USFWS database on April 4, 2023, did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. No bridges or small structures exist within the project area. Construction will occur in Fall 2026. Permanent lighting is included in the project. Temporary lighting will be used during construction.

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.6823984,-87.50993226443376,14z>



DETERMINATION KEY RESULT

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the endangered northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion (dated March 23, 2023) for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

QUALIFICATION INTERVIEW

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the northern long-eared bat^[1]?

[1] See [northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [User's Guide for the Range-wide Programmatic Consultation for Indiana Bat and Northern Long-eared Bat](#).

No

9. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

10. Does the project include slash pile burning?

No

11. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

No

12. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

13. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

14. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

15. Will the project install new or replace existing **permanent** lighting?

Yes

16. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **permanent** lighting will be installed or replaced?

Yes

17. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

18. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

19. Will the project raise the road profile **above the tree canopy**?

No

20. Is the location of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the project action area is not within suitable Indiana bat and/or NLEB summer habitat and is outside of 0.5 miles of a hibernaculum.

21. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

22. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

23. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^[1] to rate the amount of light emitted in unwanted directions?

[1] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

24. **Lighting AMM 2**

Will the **permanent** lighting be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

PROJECT QUESTIONNAIRE

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

AVOIDANCE AND MINIMIZATION MEASURES (AMMS)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

DETERMINATION KEY DESCRIPTION: FHWA, FRA, FTA PROGRAMMATIC CONSULTATION FOR TRANSPORTATION PROJECTS AFFECTING NLEB OR INDIANA BAT

This key was last updated in IPaC on July 27, 2023. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the endangered **northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [amended February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion \(dated March 23, 2023\) for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

IPAC USER CONTACT INFORMATION

Agency: Indiana Department of Transportation

Name: Ryan Falls

Address: 3650 South U.S. Highway 41

City: Vincennes

State: IN

Zip: 47591

Email: rfalls@indot.in.gov

Phone: 8125821387

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Department of Transportation



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Indiana Ecological Services Field Office
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273



In Reply Refer To:

January 10, 2024

Project Code: 2023-0110753

Project Name: Des 2101753, City of Vincennes – Washington Avenue Phase II Road
Reconstruction, Knox County

Subject: List of threatened and endangered species that may occur in your proposed project
location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/>

[s7process/index.html](https://www.fws.gov/s7process/index.html). This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process. For all **wind energy projects and projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both

migratory birds and migratory bird habitat. For information regarding the implementation of Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. **Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.**

Attachment(s):

- Official Species List
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

PROJECT SUMMARY

Project Code: 2023-0110753

Project Name: Des 2101753, City of Vincennes – Washington Avenue Phase II Road Reconstruction, Knox County

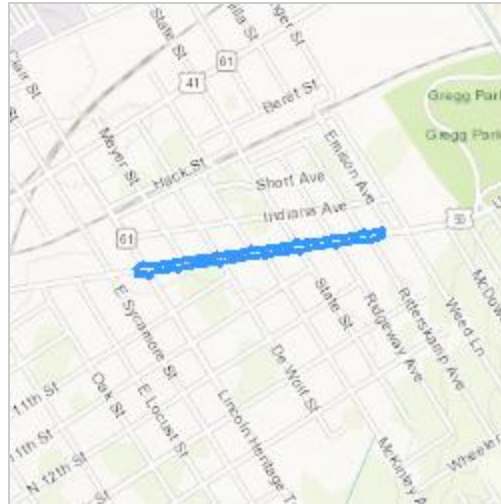
Project Type: Road/Hwy - Maintenance/Modification

Project Description: This project is located on Washington Ave, from Emison Street to East Clair Street, in T-3-N, R-10-W, Vincennes Township, Knox County, Indiana. This road reconstruction project includes the replacement and rehabilitation of the pavement on Washington Ave from E Clair St to Emison St and will consist of milling, widening, and overlay. The curb-to-curb width of the existing roadway will be widened from 40 feet(ft) to 42 ft. The existing sidewalks on both sides of Washington Ave will be widened from 5 ft to 6 ft. Additional improvements will include gutters, sidewalks, Americans with Disabilities Act (ADA) compliant curb ramps, street lighting throughout, and an updated traffic signal at the Washington Ave and St. Clair St intersection. This project will require the complete excavation and removal of the former US Highway 50 roadbed and trolley track remnants.

Suitable summer habitat is not located within or adjacent to the project. Three (3) trees are anticipated to be removed for this project. All three (3) trees are unsuitable summer habitat because they are single, ornamental trees within an urban area; and there will be no federal time of year restrictions on removal. The species of trees to be removed are Red maple (*Acer rubrum*), Silver maple (*Acer saccharinum*), and Japanese cherry (*Prunus serrulata*). A review of the USFWS database on April 4, 2023, did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. No bridges or small structures exist within the project area. Construction will occur in Fall 2026. Permanent lighting is included in the project. Temporary lighting will be used during construction.

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.6823993,-87.50992419182754,14z>



Counties: Knox County, Indiana

ENDANGERED SPECIES ACT SPECIES

There is a total of 5 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045	Endangered
Tricolored Bat <i>Perimyotis subflavus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/10515	Proposed Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

INSECTS

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

BALD & GOLDEN EAGLES

Bald and golden eagles are protected under the Bald and Golden Eagle Protection Act¹ and the Migratory Bird Treaty Act².

Any person or organization who plans or conducts activities that may result in impacts to bald or golden eagles, or their habitats³, should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the "[Supplemental Information on Migratory Birds and Eagles](#)".

1. The [Bald and Golden Eagle Protection Act](#) of 1940.
2. The [Migratory Birds Treaty Act](#) of 1918.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

There are bald and/or golden eagles in your project area.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31

NAME	BREEDING SEASON
Golden Eagle <i>Aquila chrysaetos</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1680	Breeds elsewhere

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read ["Supplemental Information on Migratory Birds and Eagles"](#), specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

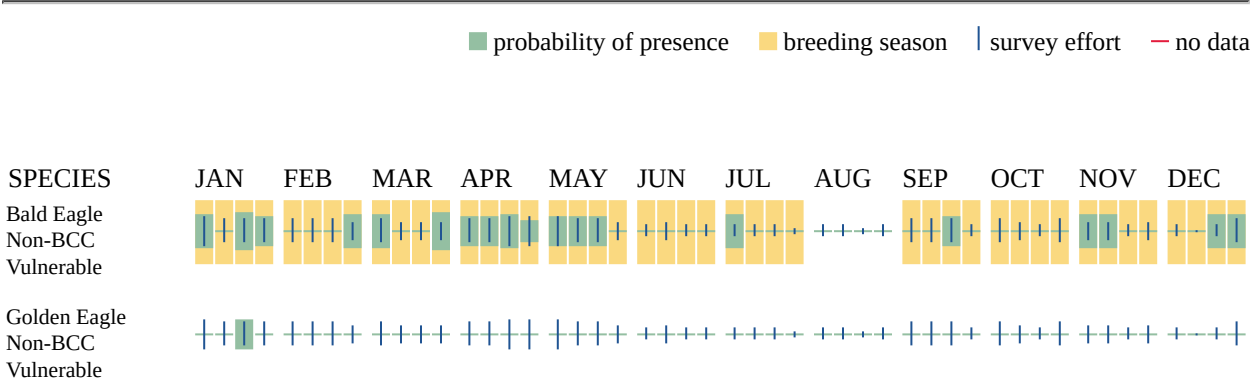
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>

- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incident-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats³ should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the "[Supplemental Information on Migratory Birds and Eagles](#)".

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31
Cerulean Warbler <i>Dendroica cerulea</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/2974	Breeds Apr 23 to Jul 20
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9406	Breeds Mar 15 to Aug 25

NAME	BREEDING SEASON
Field Sparrow <i>Spizella pusilla</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9446	Breeds Mar 1 to Aug 15
Golden Eagle <i>Aquila chrysaetos</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1680	Breeds elsewhere
Kentucky Warbler <i>Oporornis formosus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9443	Breeds Apr 20 to Aug 20
Lesser Yellowlegs <i>Tringa flavipes</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9679	Breeds elsewhere
Prairie Warbler <i>Dendroica discolor</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9513	Breeds May 1 to Jul 31
Prothonotary Warbler <i>Protonotaria citrea</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9439	Breeds Apr 1 to Jul 31
Red-headed Woodpecker <i>Melanerpes erythrocephalus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9398	Breeds May 10 to Sep 10
Rusty Blackbird <i>Euphagus carolinus</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9478	Breeds elsewhere
Wood Thrush <i>Hylocichla mustelina</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9431	Breeds May 10 to Aug 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read ["Supplemental](#)

[Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

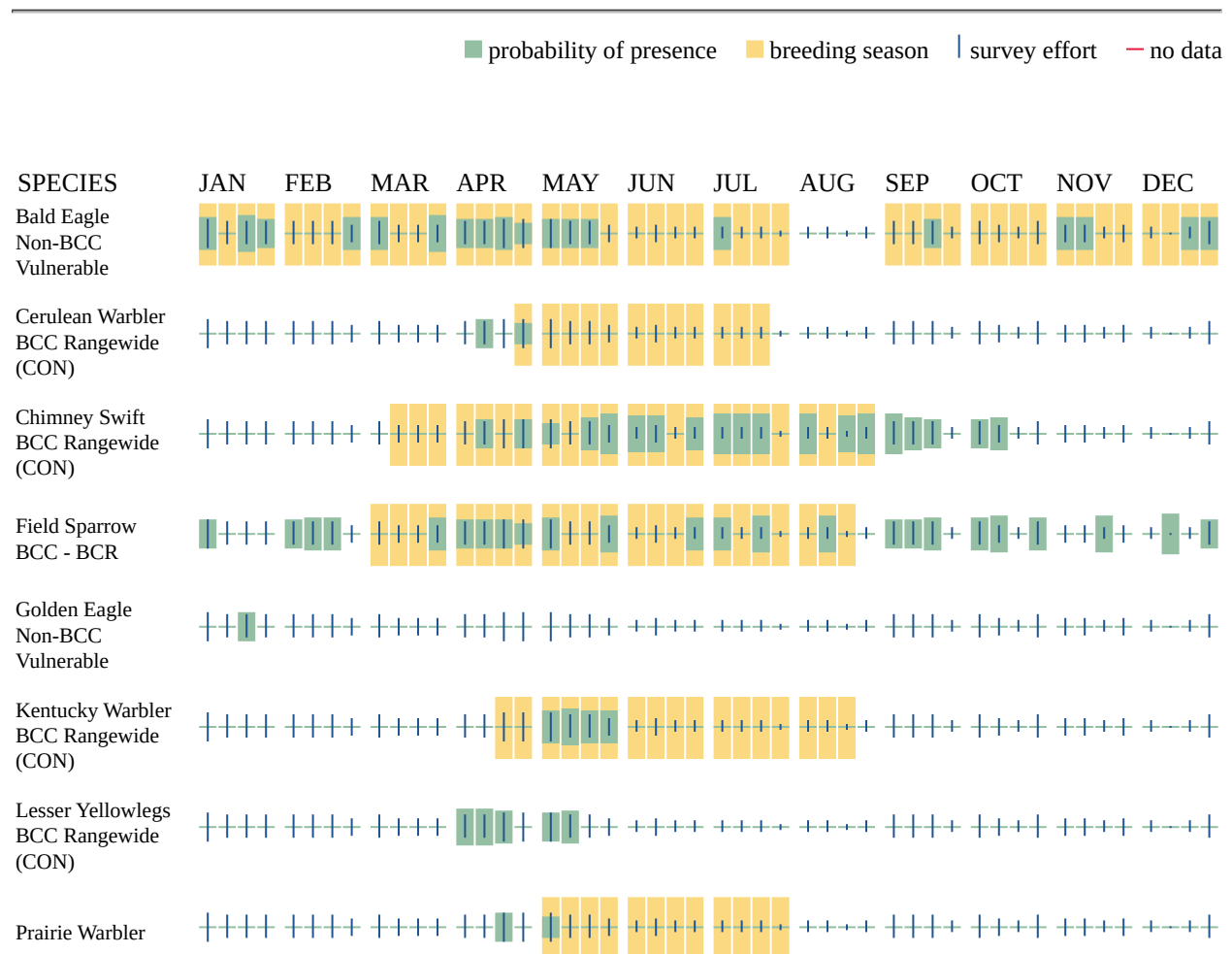
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

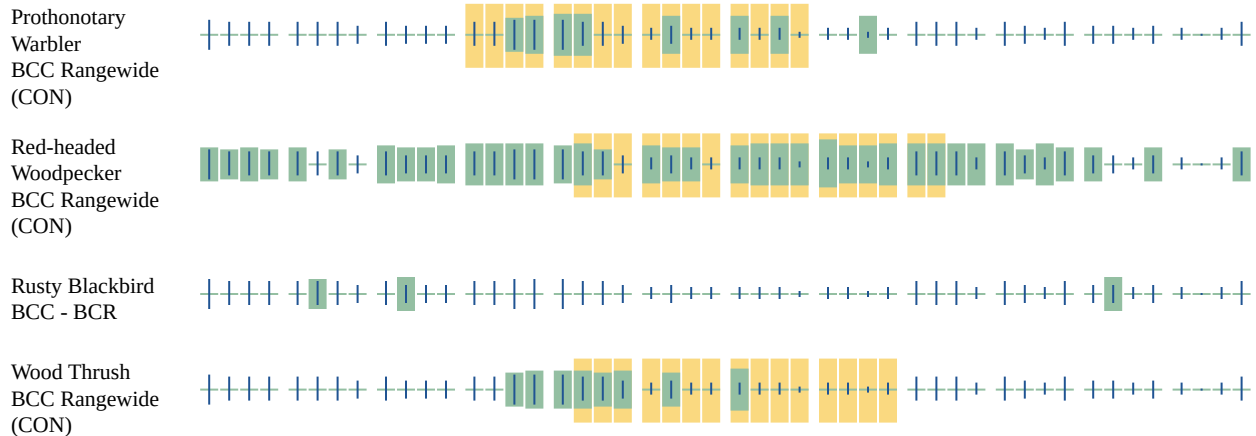
Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.



BCC Rangewide
(CON)



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

THERE ARE NO WETLANDS WITHIN YOUR PROJECT AREA.

IPAC USER CONTACT INFORMATION

Agency: SJCA Inc
Name: Laura Rogers
Address: 1104 Prospect Street
Address Line 2: Suite 201/203
City: Indianapolis
State: IN
Zip: 46203
Email: lrogers@sjcainc.com
Phone: 3175660629

LEAD AGENCY CONTACT INFORMATION

Lead Agency: Department of Transportation

Appendix D

Section 106 of the NHPA

Appendix E

Red Flag Investigation



Date: August 22, 2023

To: Site Assessment & Management (SAM)
Environmental Policy Office - Environmental Services Division (ESD)
Indiana Department of Transportation (INDOT)
100 N Senate Avenue, Room N758-ES
Indianapolis, IN 46204

From: Taylor Gabrysiak
SJCA Inc.
9102 N Meridian St, Suite 200
Indianapolis, IN 46260
tgabrysiak@sjcainc.com

Re: RED FLAG INVESTIGATION
DES #2101753, Local Project
Road Reconstruction Project
City of Vincennes – Washington Avenue (Ave) Phase II, from Emison Street (St) to East (E) Clair St
Knox County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The City of Vincennes, with partial funding from the Federal Highway Administration (FHWA), intend to proceed with the reconstruction of Washington Ave from E Clair St to Emison St, in the City of Vincennes, Knox County, Indiana. The reconstruction will involve complete pavement replacement and rehabilitation, and consist of milling, widening, and overlay. The net distance of the project is approximately 0.52 miles. The Curb Face to Curb Face width of the existing roadway will be widened from 40 feet (ft) to 42 ft. Both sides of the existing sidewalk will be widened from 5 ft to 6 ft. Additional improvements will include gutters, sidewalks, Americans with Disabilities Act (ADA) compliant curb ramps, and street lighting throughout. This project will require the complete excavation and removal of the former United States (US) Highway 50 roadbed and trolley track remnants.

Bridge Work Included in Project: Yes ☐ No ☒ Structure #(s) N/A

If this is a bridge project, is the bridge Historical? Yes ☐ No ☒ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Culvert Work Included in Project: Yes ☐ No ☒ Structure #(s) N/A

Proposed right of way: Temporary ☒ # Acres 0.90 Permanent ☒ # Acres 0.03, Not Applicable ☐

Type and proposed depth of excavation: The anticipated maximum excavation depth will be approximately 12 ft for storm sewer reconstruction. Roadway work will require approximately three (3) ft of excavation, and approximately one (1) ft of excavation is anticipated for sidewalk and curb ramp updates.

Maintenance of traffic (MOT): MOT is anticipated to be a complete road closure and detour route utilizing Niblack Road (Rd), North (N) 6th St, and College Ave. The anticipated detour will add 1.52 miles of additional travel distance.

Work in waterway: Yes ☐ No ☒ Below ordinary high water mark: Yes ☐ No ☒

State Project: ☐ LPA: ☒

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	9*	Recreational Facilities	9
Airports ¹	N/A	Pipelines	1
Cemeteries	N/A	Railroads	10
Hospitals	N/A	Trails	2
Schools	4*	Managed Lands	N/A

¹In order to complete the required airport review, a review of public-use airports within 3.8 miles (20,000 feet) is required.

Explanation:

Religious Facilities*: Nine (9) religious facilities, five (5) mapped and four (4) unmapped, are located within the 0.5 mile search radius. The nearest facility, Free Methodist Church, is located approximately 0.25 mile northwest of the project area. No impact is expected.

Schools*: Four (4) schools, three (3) mapped and one (1) unmapped, are located within the 0.5 mile search radius. One (1) school, Washington Elementary School, is located adjacent to the project area. Coordination with Washington Elementary School will occur.

Recreational Facilities: Nine (9) recreational facilities are located within the 0.5 mile search radius. One recreation facility, Washington Elementary School is located adjacent to the project area. Coordination with Washington Elementary School will occur.

Pipelines: One (1) pipeline segment is located within the 0.5 mile search radius. The segment, associated with Southern Indiana Gas & Electric Co., is located approximately 0.19 mile east of the project area. No impact is expected.

Railroads: Ten (10) railroad are located within the 0.5 mile search radius. The nearest railroad segment, CSX RR, is located approximately 0.08 mile northwest of the project area. No impact is expected.

Trails: Two (2) trail segments are located within the 0.5 mile search radius. The nearest trail segment, Vaughn Walking Trail, is located approximately 0.15 mile northeast from the project area. No impact is expected.

WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	9
Canal Structures – Historic	N/A	Lakes	N/A
NPS NRI Listed	N/A	Floodplain - DFIRM	2
NWI-Lines	10	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	2	Sinkhole Areas	N/A
Rivers and Streams	4	Sinking-Stream Basins	N/A

Explanation:

NWI-Lines: Ten (10) National Wetland Inventory (NWI) line segments are located within the 0.5 mile search radius. The nearest NWI-line segment is located approximately 0.39 mile east of the project area. No impact is expected.

IDEM 303d Listed Streams and Lakes: Two (2) 303d Listed Stream are located within the 0.5 mile search radius. The nearest IDEM 303d Listed Stream segment is located approximately 0.39 mile east of the project area. No impact is expected.

Rivers and Streams: Four (4) river and stream segments are located within the 0.5 mile search radius. The nearest river and stream segment, associated with Kelso Creek, is located approximately 0.39 mile east of the project area. No impact is expected.

NWI-Wetlands: Nine (9) wetlands are located within the 0.5 mile search radius. The nearest wetland polygon is located approximately 0.44 mile southwest of the project area. No impact is expected.

Floodplains: Two (2) floodplain polygons are located within the 0.5 mile search radius. The nearest floodplain polygon is located approximately 0.37 mile east of the project area. No impact is expected.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	1	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	2

Explanation:

Petroleum Wells: One (1) petroleum well is located within the 0.5 mile search radius. The nearest petroleum well is located approximately 0.42 mile northeast of the project area. No impact is expected.

Mines – Underground: Two (2) underground mines are located within the 0.5 mile search radius. The nearest underground mine is located approximately 0.12 mile north of the project area. No impact is expected.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	7	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	1	Waste Transfer Stations	1
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	14	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	1
Construction Demolition Waste	N/A	Institutional Controls	3
Solid Waste Landfill	N/A	NPDES Facilities	2
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A
Leaking Underground Storage (LUST) Sites	15	Notice of Contamination Sites	N/A

Unless otherwise noted, site specific details presented in this section were obtained from documents reviewed on the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC).

Explanation:

RCRA Generator/ TSD: Seven (7) Resource Conservation and Recovery Act (RCRA) Generator/ TSD sites were identified within the 0.5 mile search radius. The nearest facility, Former Fields Cleaners, 1401 Washington Ave, AI ID# 30507, is located 0.03 mile west of the project area. According to the February 2, 2002 Hazardous Waste Handle Identification Form, the facility was classified as a Conditionally Exempt Small Quantity Generator. This site is an active dry cleaners that is currently in IDEM's Voluntary Remediation Program (VRP). Chlorinated solvent contamination remains on site in soils and groundwater contamination extends southwest off the site. IDEM's July 12, 2023 letter indicates that contamination has been delineated and plume stability is being evaluated for closure. If excavation occurs in this area, it is possible chlorinated solvent contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

State Cleanup Sites: One (1) State Cleanup site is located within the 0.5 mile search radius. Former Phillip 66, 2232 N 6th St, AI ID# 36047, is located 0.38 mile north from the project area and was formerly the site of a gas station. According to the No Further Action (NFA) Determination Pursuant to Risk Integrated System of Closure (RISC) issued by IDEM on June 24, 2021, contamination remains in the area surrounding the site and exists in the ROW. No impact is expected.

Underground Storage Tank (UST) Sites: Fourteen (14) UST sites are located within the 0.5 mile search radius. One (1) UST site, Hucks 135, 1515 Washington Ave, AI ID# 31038, is located within the project area. IDEM conducted an Underground Storage Tank inspection on November 28, 2020, and the facility was found to be out of compliance with equipment, operating, and maintenance requirements set forth in Indiana's UST Rule 329 IAC 9; however, documentation reviewed does not indicate that a release occurred. IDEM issued a Return to Compliance Letter on January 8, 2021. No impact is expected.

Leaking Underground Storage (LUST) Sites: Fifteen (15) LUST sites are located within the 0.5 mile search radius. Four (4) LUST sites are located adjacent or within the project area.

- Genes 76, 1530 Washington Ave, AI ID# 33620, is located within the project area and was formerly the site of a gas station. According to the UST Closure Report dated March 26, 1999, the USTs were removed from the site after a confirmed release on March 9, 1999. IDEM issued an NFA on November 28, 2001, stating that total petroleum hydrocarbons (TPH) were below the detection limit of 20 parts per million (ppm). No impact is expected.
- Cantwell Service Center, 1602 Washington Ave, AI ID# 33752, is located within the project area and is an active gas station and service center. IDEM issued a NFA Approval Determination Pursuant to Remediation Closure Guide for the site on July 30, 2020. IDEM conducted an Underground Storage Tank Inspection on August 9, 2022, and the facility was found to be out of compliance with equipment, operating, and maintenance requirements set forth in Indiana's UST Rule 329 IAC 9; however, documentation reviewed does not indicate that a release occurred. IDEM issued a Return To Compliance Letter for the site on May 15, 2023. No impact is expected.
- Swifty Service Station #192, 1842 Washington Ave, AI ID# 31441, is located within the project area and was formerly the site of a gas station. According to the LUST Quarterly Monitoring Report dated April 7, 2010, petroleum contamination in soil was below the Risk Integrated System of Closure (RISC) migration to groundwater industrial default closure level (IDCL). Groundwater contamination was below the RISC residential default closure level (RDCL) except for one well that was below the RISC IDCL for benzene. IDEM issued a NFA Further Action Determination Pursuant to 1994 UST Guidance on July 6, 2010. If excavation occurs in this area, it is possible that petroleum contamination may be encountered. Proper handling, removal, and disposal of soil and/or groundwater will be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.
- Bus Barn/Maintenance Shop, 2120 Washington Ave, AI ID # 34277, is located adjacent to the project area. IDEM issued a No Further Action (NFA) Determination Pursuant to 1994 UST Branch Guidance for the site on January 18, 2006. Soil and groundwater petroleum contamination above the 1994 Guidance Corrective Action Guidelines remained on site. If excavation occurs in this area, it is possible chlorinated solvent contamination may be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

Waste Transfer Stations: One (1) Waste Transfer Station is located within the 0.5 mile search radius. Eastern Environmental Service Transfer Station, 1640 N 6th St, AI ID# 36193, is located 0.21 mile north from the project area. IDEM issued a Notice of Closure and Certification of Closure on December 8, 1999. No impact is expected.

Brownfields: One (1) Brownfield site was identified within the 0.5 mile search radius. Walters Property, 1237 Washington Ave, AI ID# 31723, is located 0.10 mile west from the project area. An Environmental Restrictive Covenant (ERC) was placed on the property on June 3, 2015. No impact is expected.

Institutional Controls: Three (3) Institutional Controls sites are located within the 0.5 mile search radius. The nearest institutional control site, R G Properties & Development, LLC, 1216 Washington Ave, AI ID# 30281, is located 0.08 mile west from the project area. An Environmental Restrictive Covenant (ERC) was placed on the property August 23, 2013. No impact is expected.

NPDES Facilities: Two (2) National Pollutant Discharge Elimination System (NPDES) Facilities were identified within the 0.5 mile search radius. The nearest NPDES Facility, Dumes Inc., 1640 N 6th St, INRM00765, is located 0.20 mile north from the project area. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Knox County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is provided at https://www.in.gov/dnr/nature-preserves/files/np_Knox.pdf. A preliminary review of the Indiana Natural Heritage Database by INDOT ESD did indicate the presence of ETR species within the 0.5 mile search radius. Due to the nature of project activities, this project will fall under the guidelines set forth under the USFWS Interim Policy for the Review of Highway Transportation Projects in Indiana dated May 29, 2013. No further coordination is necessary.

A review of the USFWS database did not indicate the presence of endangered bat species within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Schools: Washington Elementary School is located adjacent to the project area. Coordination with Washington Elementary School will occur.

Recreation Facilities: Washington Elementary School is located adjacent to the project area. Coordination with Washington Elementary School will occur.

WATER RESOURCES: N/A

MINING/MINERAL EXPLORATION: N/A

HAZARDOUS MATERIAL CONCERNS:

RCRA Generator/TSD: Former Fields Cleaners is located 0.03 mile west of the project area. This site is an active dry cleaners that is currently in IDEM's Voluntary Remediation Program (VRP). Chlorinated solvent contamination remains on site in soils and groundwater contamination extends southwest off the site. If excavation occurs in this area, it is possible chlorinated solvent contamination will be encountered. Proper

handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

LUST Sites:

- Swifty Service Station #192, 1842 Washington Ave, AI ID#31441, is located within the project area and was formerly the site of a gas station. IDEM issued a No Further Action Determination Pursuant to 1994 UST Guidance on July 6, 2010, but the entire letter is not viewable. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater will be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.
- Bus Barn/Maintenance Shop, 2120 Washington Ave, AI ID #34277, is located adjacent to the project area. IDEM issued a No Further Action (NFA) Determination Pursuant to 1994 UST Branch Guidance for the site on January 18, 2006. Soil and groundwater petroleum contamination above the 1994 Guidance Corrective Action Guidelines remained on site. If excavation occurs in this area, it is possible chlorinated solvent contamination may be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Refer to Appendix G of the SAM Manual for the recommended procedure to manage and report contamination.

ECOLOGICAL INFORMATION: The range-wide programmatic consultation for the Indiana Bat and Northern Long-Eared Bat will be completed according to the most recent “Using the USFWS’s IPaC System for Listed Bat Consultation for INDOT Projects”.

INDOT ESD concurrence: Nicole Fohey-Breting (Signature)
Digitally signed by Nicole Fohey-Breting
Date: 2023.08.24 04:01:33 -04'00'

Prepared by:
Taylor Gabrysiak
GIS Specialist
SJCA Inc.

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

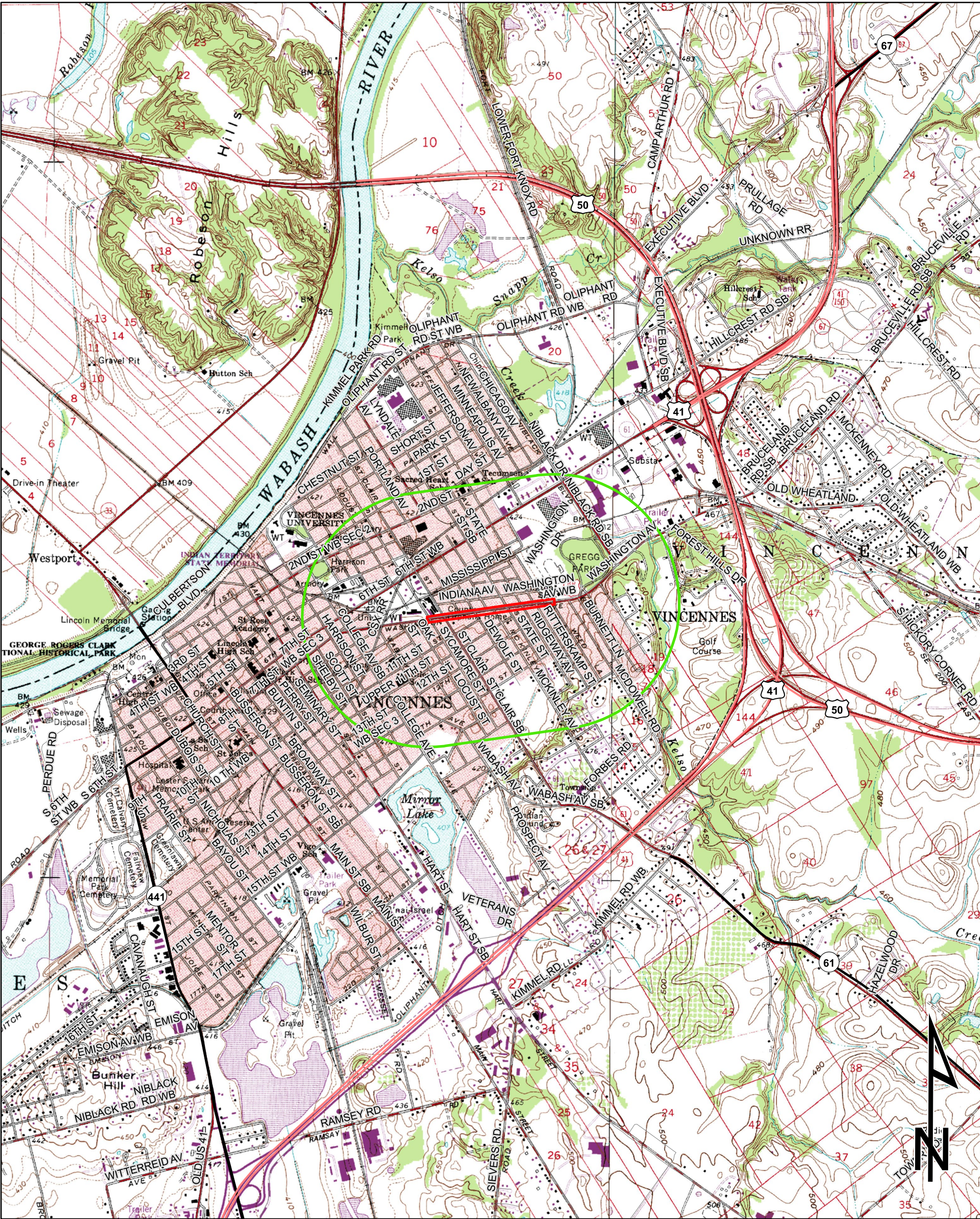
INFRASTRUCTURE: YES

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: YES

HAZARDOUS MATERIAL CONCERNS: YES

Red Flag Investigation - Site Location
City of Vincennes - Washington Avenue Phase II, from Emison Street to East Clair Street
Des. No. 2101753, Road Reconstruction Project
Knox County, Indiana

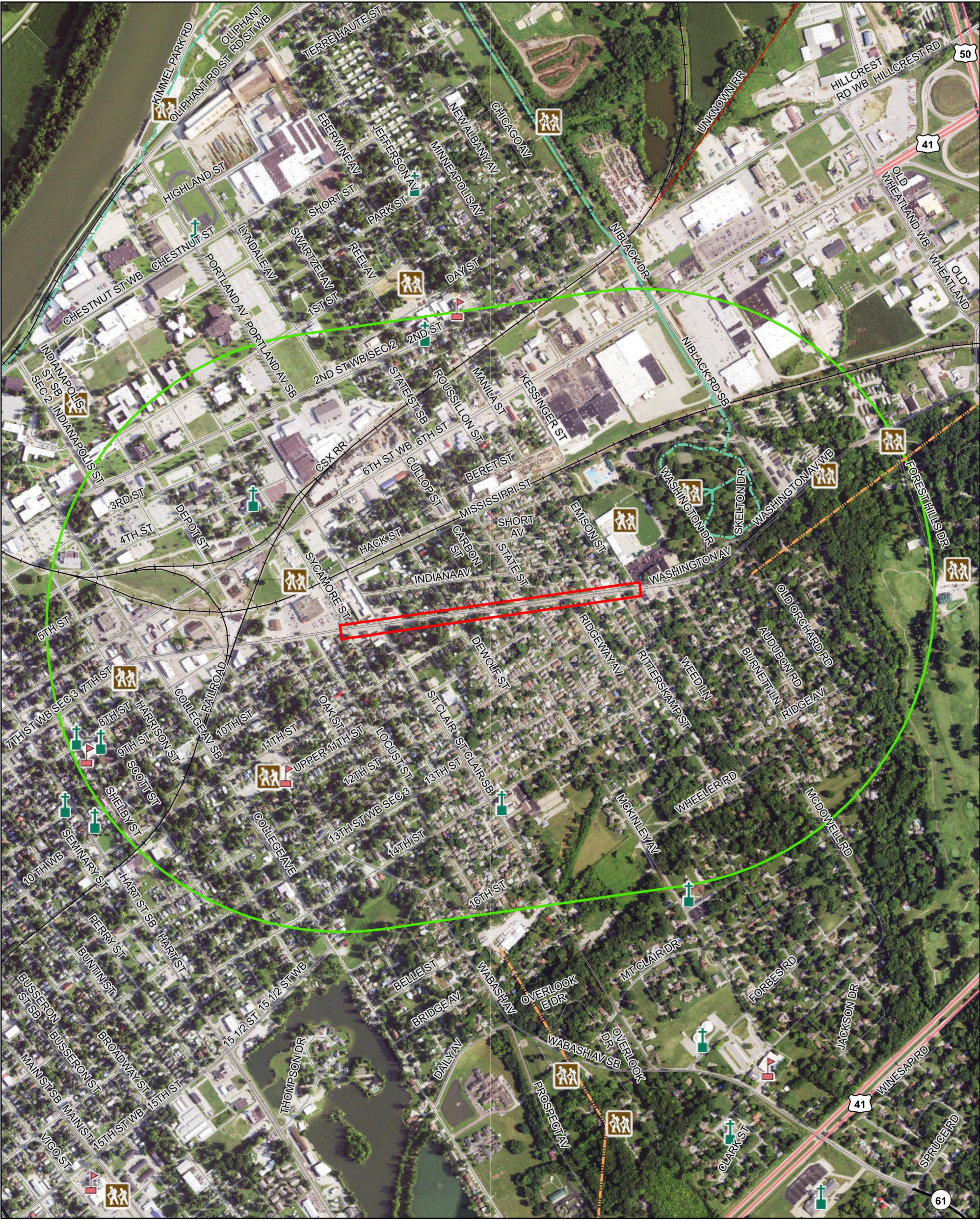


Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

FRITCHTON AND VINCENNES
QUADRANGLE
INDIANA
7.5 MINUTE SERIES
(TOPOGRAPHIC)

Red Flag Investigation - Infrastructure

City of Vincennes - Washington Avenue Phase II, from Emison Street to East Clair Street
Des. No. 2101753, Road Reconstruction Project
Knox County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

0 0.1 0.2 0.4 Miles

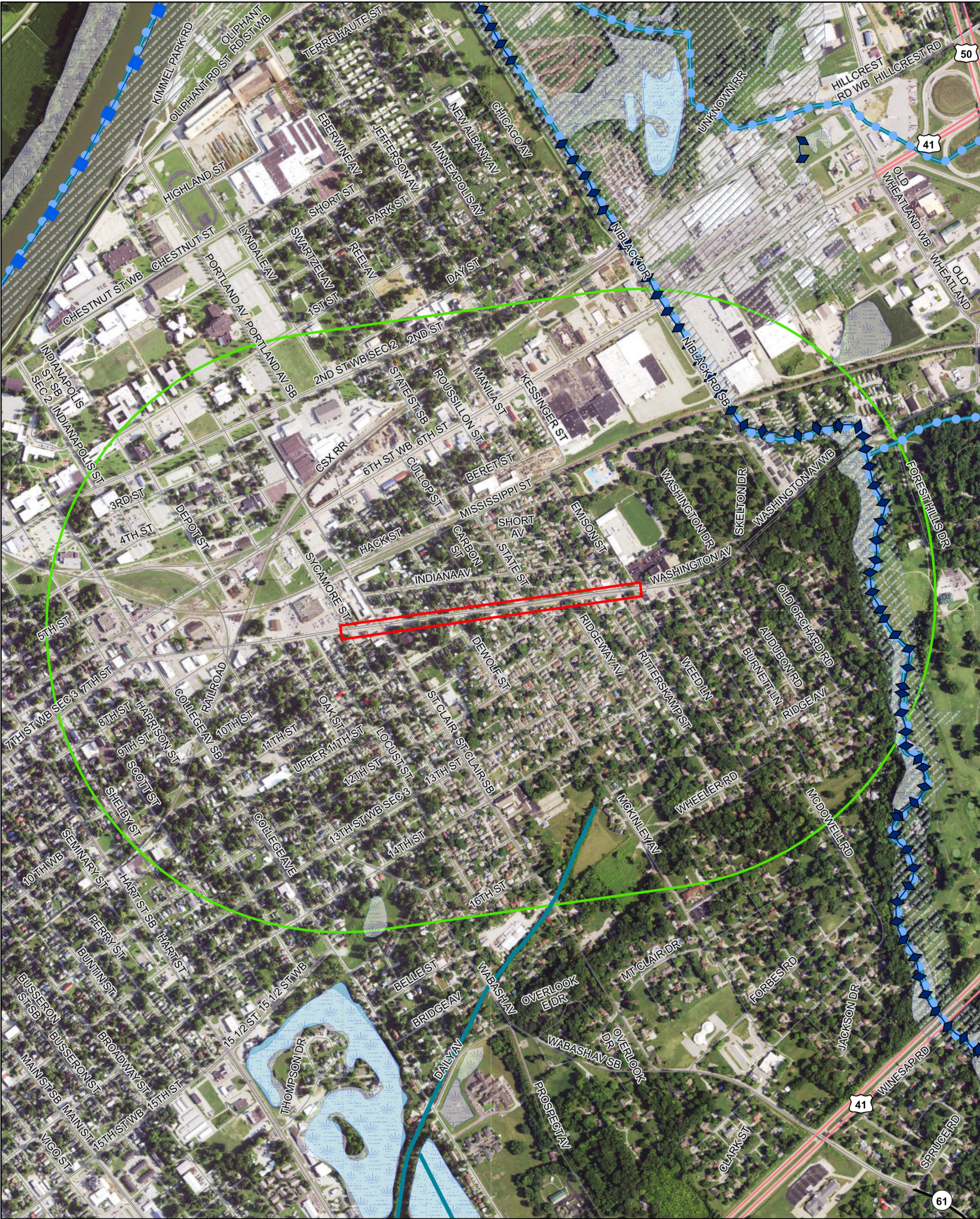
	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

Red Flag Investigation - Water Resources

City of Vincennes - Washington Avenue Phase II, from Emison Street to East Clair Street

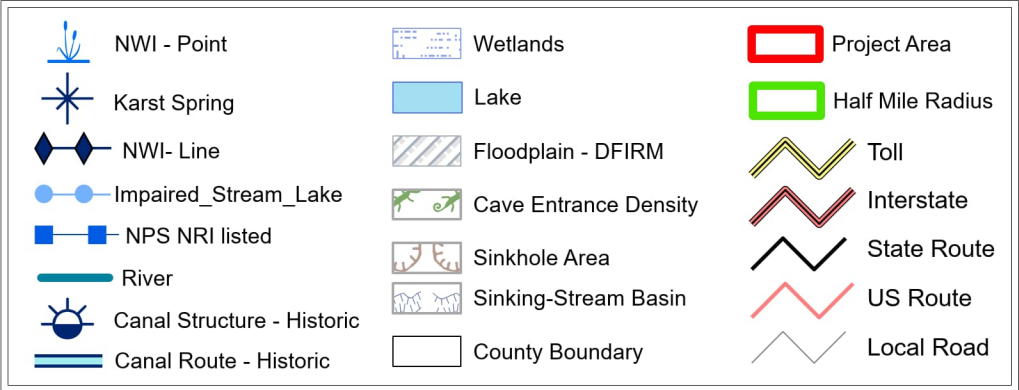
Des. No. 2101753, Road Reconstruction Project

Knox County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

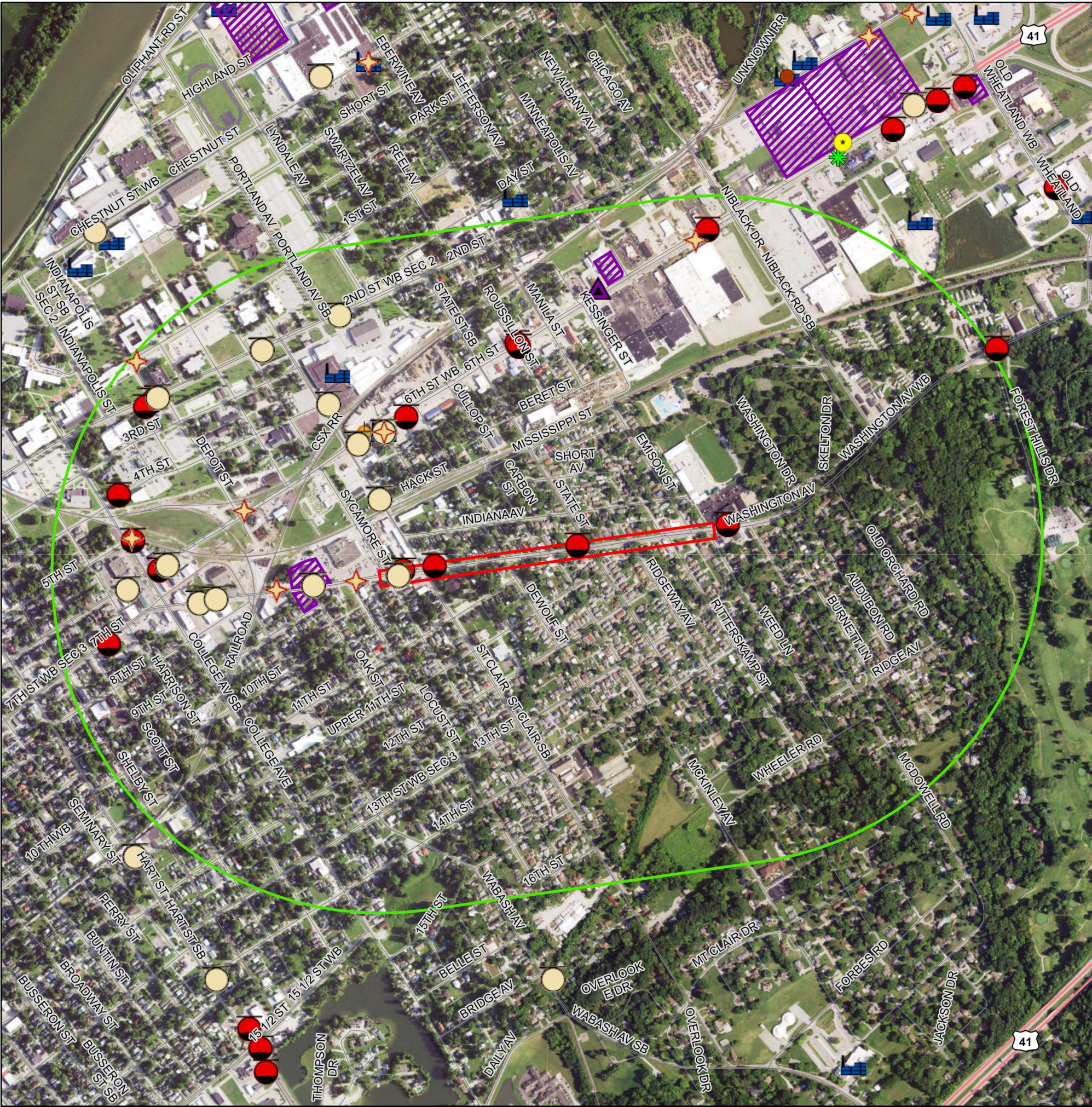


Red Flag Investigation - Hazardous Material Concerns

City of Vincennes - Washington Avenue Phase II, from Emison Street to East Clair Street

Des. No. 2101753, Road Reconstruction Project

Knox County, Indiana



Brownfield

RCRA Corrective Action Sites

Confined Feeding Operation

Notice Of Contamination

Construction/Demolition Site

Infectious/Medical Waste Site

Leaking Underground Storage Tank

Manufactured Gas Plant

NPDES Facilities

NPDES Pipe Locations

Open Dump Waste Site

RCRA Generator/TSD

Restricted Waste Site

Septage Waste Site

Solid Waste Landfill

State Cleanup Site

Superfund

Tire Waste Site

Underground Storage Tank

Voluntary Remediation Program

Waste Transfer Station

Institutional Controls

County Boundary

Project Area

Half Mile Radius

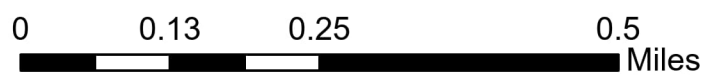
Toll

Interstate

State Route

US Route

Local Road



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

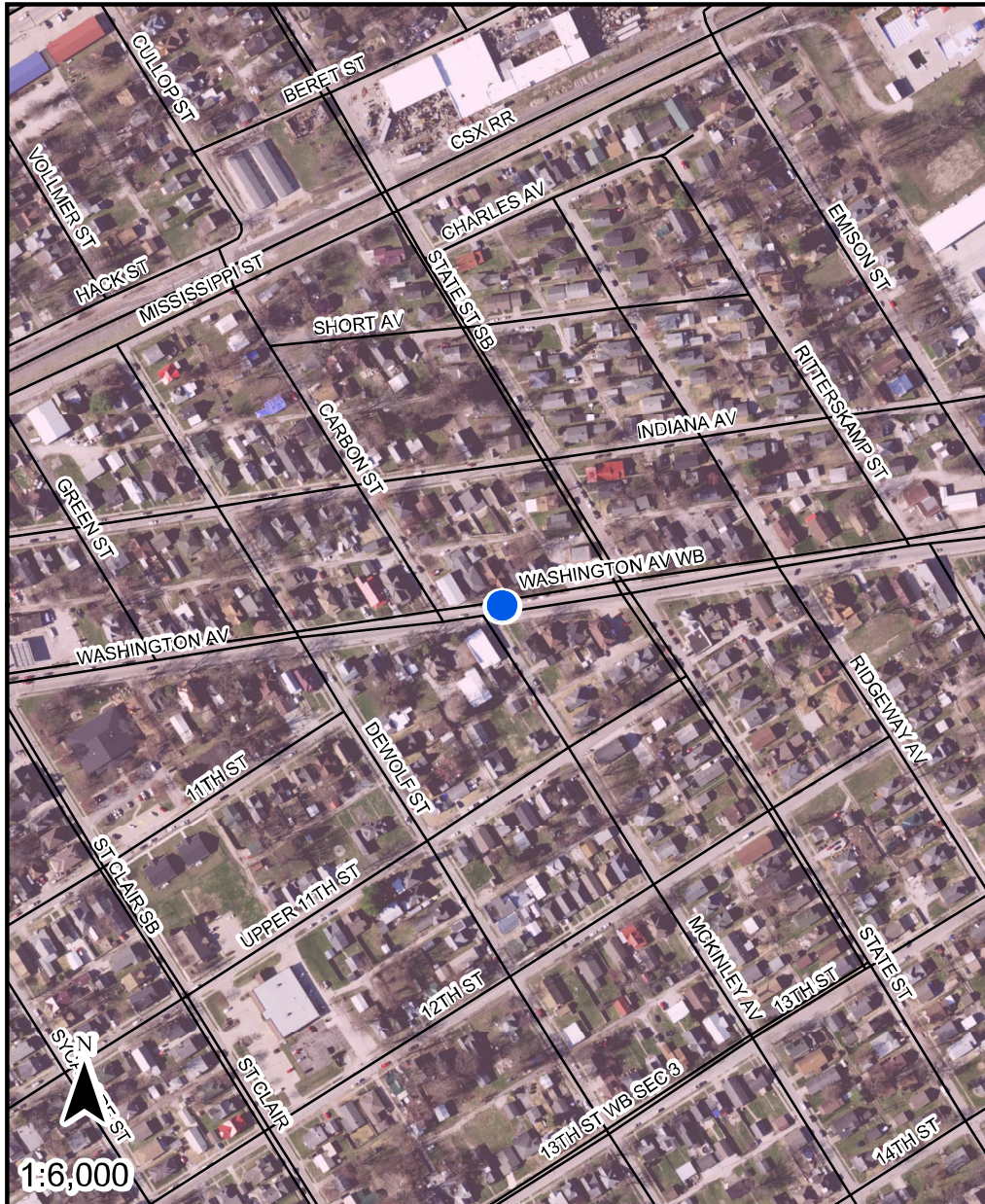
Map Projection: UTM Zone 16 N

Map Datum: NAD83

E12

Appendix F

Water Resources



- Point of Interest
- Base Flood Elevation Point
- Additional Floodplain Area; DNR .2 Percent Flood Hazard

Point of Interest Coordinates
(WGS84)

Long: **-87.508779689**

Lat: **38.6824933591**

The information provided below is based on the point of interest shown in the map above.

County: **Knox**

Approximate Ground Elevation: **422.3 feet (NAVD88)**

Stream Name:

Base Flood Elevation: **429.1 feet (NAVD88)**

Kelso Creek

Drainage Area: **Not available**

Best Available Flood Hazard Zone: **0.2 PCT ANNUAL CHANCE FLOOD HAZARD**

National Flood Hazard Zone: **0.2 PCT ANNUAL CHANCE FLOOD HAZARD**

Is a Flood Control Act permit from the DNR needed for this location? **See following pages**

Is a local floodplain permit needed for this location? **Contact your local Floodplain Administrator-**

Floodplain Administrator: **Colt Michaels, Director, Area Plan Commission**

Community Jurisdiction: **City Of Vincennes, City proper**

Phone: **(812) 885-2544**

Email: **cmichaels@knoxcounty.in.gov**

US Army Corps of Engineers District: **Louisville**

Date Generated: 8/24/2023

Appendix G

Public Involvement



NOTICE OF SURVEY

November 22, 2022

RE: PROJECT: Washington Avenue from E. St. Clair St. to Emison Ave. (Phase II)
Road Reconstruction Project.
Vincennes, Indiana

Dear Property Owner:

Our information indicates that you own or occupy property near this proposed (Phase II) Road Reconstruction Project. Our employees will be doing a survey of the project area in the near future. It may be necessary for them to come onto your property to complete this work. This is allowed by Indiana Code IC 8-23-7-26. They will show you their identification, if you are available, before coming onto your property. If you have sold this property, or someone else occupies it, please let us know the name and address of the new owner or current occupant so we can contact them about the survey.

At this stage we generally do not know what effect, if any, our project may eventually have on your property. If we determine later your property is involved, we will contact you with additional information.

The survey work will include mapping the location of features such as buildings, trees, fences, and drives, and obtaining ground elevations. This work is necessary for the proper planning and design of the (Phase II) Road Reconstruction Project. Please be assured of our sincere desire to cause you as little inconvenience as possible during the survey. If any problems do occur, please contact our field crew, or contact me at the phone number or address shown below.

We do appreciate your input regarding any issues that this project may encounter during the design phase. Included with this notice is a short questionnaire (on back) that you can fill out and return to us in the enclosed self-addressed stamped envelope. Thank you, in advance, for your participation in this process.

Sincerely,

SJCA Inc.

Corey A. Chenault

Corey A. Chenault
Survey Crew Coordinator
CChenault@SJCAinc.com
317-566-0629 ext 441



Certified MBE, State of Indiana; City of Indianapolis

INDOT Certified DBE

Job #20SU061

SURVEY QUESTIONNAIRE

November 22, 2022

RE: PROJECT: Washington Avenue from E. St. Clair St. to Emison Ave. (Phase II)
Road Reconstruction Project.
Vincennes, Indiana

Name of person completing questionnaire: _____

Have you received the Notice of Survey letter? (yes or no): _____

If different from the letter, the correct occupant's name and address should be:

Name: _____

Address: _____

If you have any special requests (instructions to close gates, beware of dog, etc.), please list here:

Please describe any areas where you feel there may be stormwater problems (e.g. flooding, clogged pipes, standing water, etc.)

If the property utilizes water wells and/or septic systems, please describe their location: _____

Please describe any facilities that are underground and not visible: _____

Any other issues we should be aware of? _____

Please return this page only.

Appendix H

Air Quality

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2024 - 2028

SPONSOR	CONTR ACT # / LEAD DES	STP NAME	ROUTE	WORK TYPE	DISTRICT	MILES	FEDERAL CATEGORY	Total Cost of Project*	PROGRAM	PHASE	FEDERAL	MATCH	2024	2025	2026	2027	2028
Performance Measure Impacted: Pavement Condition																	
Location: From SR 58 to SR 67																	
Comments Include DES 200056																	
Indiana Department of Transportation	A151 / 1000082	Ind.	US 50	HMA Functional Overlay on PCCP	Vincennes	3.460	NHPP	\$10,705,000.00	Safety Construction	CN	\$7,842,400.00	\$710,600.00		\$0.00	\$3,863,000.00		
									Road Construction	CN	\$0,344,000.00	\$2,086,000.00		\$0.00	\$10,430,000.00		

Location: From E. Jct of US-41 SRL to 4.75 east of E. Jct of US-41 SRL																	
Comments Include DES 1500062, 1902863																	
Indiana Department of Transportation	A3227 / 2001639	Ind.	SR 67	Small Structure Replacement	Vincennes	0	STBG	\$896,000.00	Bridge Construction	CN	\$559,200.00	\$139,800.00		\$696,000.00			
Performance Measure Impacted: Bridge Condition																	
Location: 2+63 N JCT SR 358																	
Comments Include DES 2001939																	
Indiana Department of Transportation	A3228 / 2002051	Ind.	SR 61	Bridge Deck Overlay	Vincennes	0	STBG	\$0,210,000.00	Bridge Construction	CN	\$4,281,600.00	\$1,076,400.00		\$5,352,000.00			
Performance Measure Impacted: Bridge Condition																	
Location: over WHITE RIVER OVERFLOW, 02.09 N SR 56																	
Comments Include DES 2002050, 2002051, 2002060																	
Indiana Department of Transportation	A3230 / 2002040	Ind.	A 02 US 50	Bridge Deck Replacement	Vincennes	0	NHPP	\$2,652,250.00	Bridge Consulting	PE	\$100,560.00	\$25,140.00	\$13,700.00	\$112,000.00			
Performance Measure Impacted: Bridge Condition																	
Location: EB over CSX RR, FT KNOX ROAD, 01.24 W US 41																	
Comments Include PE FY2024 and FY2025																	
Indiana Department of Transportation	A3981 / 2100258	Ind.	US 41	HMA Overlay, Preventive Maintenance	Vincennes	11.253	NHPP	\$70,288,000.00	Road Construction	CN	\$14,710,400.00	\$3,677,600.00		\$10,000.00	\$18,378,000.00		
Performance Measure Impacted: Pavement Condition																	
Location: From 0.50 miles S of SR-241 to 0.58 mi S of E intersection with US 50 (Kelso Creek bridge)																	
Comments Include DES 2100258																	

Vincennes	A4285 / 2101793	Ind.	ST 67-21	HMA Overlay, Structural	Vincennes	.42	STBG	\$5,102,000.00	Group III Program	CN	\$3,074,000.00	\$0.00					\$3,074,000.00
									Local Funds	ROW	\$0.00	\$156,000.00		\$156,000.00			
									Local Funds	CN	\$0.00	\$769,000.00				\$769,000.00	
									Group III Program	ROW	\$624,000.00	\$0.00		\$624,000.00			

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*Estimated Cost to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2024 - 2028

SPONSOR	CONTR ACT # / LEAD DES	STP NAME	ROUTE	WORK TYPE	DISTRICT	MILES	FEDERAL CATEGORY	Total Cost of Project*	PROGRAM	PHASE	FEDERAL	MATCH	2024	2025	2026	2027	2028
Performance Measure Impacted: Pavement Condition																	
Location: Washington Ave. Phase II from the bounds of Phase I at Enison Ave. to St. Clair Street																	
Comments Include DES 2101753																	
Indiana Department of Transportation	A4491 / 2200499	Ind.	US 41	Bridge Deck Replacement	Vincennes	0	NHPP	\$2,395,000.00	Bridge Consulting	PE	\$186,400.00	\$46,600.00	\$233,000.00				
									Bridge Construction	CN	\$1,729,600.00	\$432,400.00			\$20,000.00	\$2,142,000.00	
Performance Measure Impacted: Bridge Condition																	
Location: Old SR 61 (Viasash Ave) Over US 41 2.07 miles S US 50																	
Comments Include DES 2200469																	
Indiana Department of Transportation	A4498 / 2200579	Ind.	SR 61	HMA Overlay, Preventive Maintenance	Vincennes	5.694	STBG	\$5,222,000.00	Road ROW	ROW	\$65,600.00	\$16,400.00		\$82,000.00			
									Road Construction	CN	\$3,766,000.00	\$942,000.00			\$76,000.00	\$4,836,000.00	
									Road Consulting	PE	\$344,000.00	\$86,000.00	\$430,000.00				
Performance Measure Impacted: Pavement Condition																	
Location: From 1.10 miles N of Jct with SR 36 SR 57 to 0.83 miles S of S Jct with SR 241																	
Comments Include DES 2200979																	
Indiana Department of Transportation	A4510 / 2200954	Ind.	US 50	Bridge Painting	Vincennes	0	NHPP	\$3,365,128.00	Bridge Construction	CN	\$2,250,400.00	\$562,600.00		\$90,000.00		\$2,723,000.00	
									Bridge Consulting	PE	\$441,600.00	\$110,400.00	\$552,000.00				
Performance Measure Impacted: Bridge Condition																	
Location: EB over White River Overflow 02.77 miles E of SR 241																	
Comments Include DES 2200654, 2200655, 2200656, 2200657, 2200659, 2200961																	
Indiana Department of Transportation	A4524 / 2200105	Ind.	A 01 SR 58	Bike/Pedestrian Facilities	Vincennes	.69	STBG	\$3,791,552.00	Safety Consulting	PE	\$644,821.80	\$161,230.40	\$806,152.00				
Performance Measure Impacted: Reliability and Freight Reliability																	
Location: From 0.90 miles E of SR 159 to W Jct with SR 67																	
Comments PE FY2024 \$806,152																	
Indiana Department of Transportation	A4522 / 2300779	Ind.	A 02 US 41	Bridge Thin Deck Overlay	Vincennes	0	NHPP	\$6,641,000.00	Road Consulting	PE	\$467,200.00	\$116,800.00	\$554,000.00				
									Bridge Consulting	PE	\$394,400.00	\$98,600.00	\$493,000.00				
									Road ROW	ROW	\$20,000.00	\$5,000.00			\$25,000.00		

Page 131 of 315 Report Created: 12/22/2023 11:21:16AM

*Estimated Cost to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Appendix I

Additional Studies and Information

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated March 2022)

ProjectNumber	SubProjectCode	County	Property
1800197	1800197	Knox	Four Lakes Park
1800278	1800278	Knox	Sandborn Community Park
1800344	1800344	Knox	Ouabache Trails Park
1800589	1800589	Knox	Fox Ridge Nature Park

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.



AMERICAN
STRUCTUREPOINT
INC.

9025 RIVER ROAD, SUITE 200
INDIANAPOLIS, INDIANA 46240
TEL 317.547.5580
FAX 317.543.0270

ABBREVIATED ENGINEER'S ASSESSMENT
WASHINGTON AVENUE RECONSTRUCTION PHASE 2
ROADWAY IMPROVEMENT PROJECT
Vincennes, Indiana

For Submittal to
INDOT

April 2023

Des No.: 2101753

Abbreviated Engineer's Assessment

Type of Work: Reconstruction (Non-Freeway)
Route: Washington Avenue
Functional Classification: Major Collector
City/County: Vincennes/Knox
Posted Speed Limit: Washington Avenue – 30/20 mph

Project Location

The project is located at the intersection of Washington Avenue and St. Clair Street, extending approximately 2,165 ft to the limits of Phase 1 project at the intersection of Washington Avenue and Emison Avenue. The project is located within Vincennes, Indiana, Section Survey 20, T 3 N, R 10 W, Vincennes Township, Knox County. See Appendix A for location map.

Project Need and Purpose

The need for this project is to improve the pavement structure, drainage, pedestrian accessibility, parking, and safety of the roadway. This project includes pavement reconstruction, the addition of delineated parking, shared roadway markings and signage, lighting, and ADA compliant curb ramps and sidewalk along the corridor. Additionally, the improvements to the signalized intersection at St. Clair Street and Washington Avenue include new traffic signal mast arms, safer pedestrian signals and marked crosswalks, and longer turn lane bays to allow for more vehicular storage.

Existing Facility

Washington Avenue is classified as an Urban Major Collector with a posted speed limit of 30 mph from St. Clair Street to Ridgeway Avenue, and 20 mph from Ridgeway Avenue to Emison Avenue. The corridor is comprised of a 40 ft roadway delineated with only a double yellow line and deteriorating curb and non-delineated parking. The existing terrain is level along the corridor and the stormwater runoff is collected by a storm sewer system with inlets. No existing roadside ditches are present. A 5 ft sidewalk is present on both sides of the roadway and is in poor condition with curb ramps that are not ADA compliant. The pavement is predominantly HMA in fair condition with some fatigue cracking, block cracking, and rutting in the wheel paths. The existing pavement section contains remnants of Old US 50 Highway roadbed as well as rail ties, bricks, and concrete from the old trolley rail system.

Existing storm sewer is present along with existing water main, sanitary sewer, and gas along the corridor. Lighting is present along the corridor and is located on the utility poles. The immediate land use is a mix of residential and commercial uses with an existing right-of-way width of 60 ft.

Traffic Data

Street Name	Washington Avenue
AADT 2027	9,900
AADT 2047	17,900
DHV 2047	1,790
Truck %	13% AADT

Design Data

Street Name	Washington Avenue
Posted Speed/Design Speed	30 and 20 mph/30 mph
Project Design Criteria	4R Reconstruction (Non-Freeway)
Functional Classification	Major Collector
Rural/Urban - Terrain	Urban - Level
Access Control	None

Identification of Proposal

The existing horizontal alignment will not be altered. The proposed vertical profile will be developed to offer adequate drainage of stormwater conveyance, minimize the need for right-of-way acquisition, and eliminate the need for any relocations. The roadway corridor will be widened from 40 ft to 42 ft from curb face to curb face, curb and gutter will be added on the intersecting minor streets, but not widened, and the intersection radii will remain consistent with existing to reduce the need for right-of-way acquisition. All pedestrian facilities will be in accordance with ADA Standards and curb ramps will be added at all intersections. The approximate length of construction along Washington Avenue from St. Clair Street to the limits of Phase 1 at Emison Avenue will be 2,165 ft.

The proposed lane configuration consists of one – 13 ft sharrows lanes (bicycle and vehicular traffic) in each direction. There will be 8 ft wide parking on both sides (6 ft pavement width plus 2 ft gutter width). Finally, this corridor will consist of a 6 ft wide sidewalk on both sides with street lighting. See Appendix B for roadway typical sections. All signage and pavement markings will be updated and in accordance with the latest version of the INMUTCD.

No Level One design exceptions are anticipated for this project.

Estimated Costs:

The preliminary construction cost estimate for the project is \$3,774,000 with a contingency percentage of 15%,

	(2023)
Preliminary Engineering (2023):	= \$478,000
Right-of-Way Acquisition (2025):	= \$780,500
Construction (2027):	= \$3,774,000
Construction Inspection (2027):	= \$453,000

Total Cost

= \$5,485,500

Environmental Issues

The anticipated environmental impacts associated with this project will include three leaking underground storage tanks and one underground storage tank requiring coordination with IDEM. The presence of limestone curbs, an old trolley rail system, and impacts of the old Knox County Orphan's home will require CRO, SHPO, and Archaeological investigations. No waterways are found in this area therefore no waterway permits are anticipated. A Construction Stormwater General Permit is anticipated to be required for the completion of this project.

Right-of-Way Impact

The land use within and around the project limits consists of residential and commercial areas. The proposed intersection at Washington Avenue and St. Clair Street will require permanent right-of-way from the adjacent parcel in the northwest corner of the intersection. It is anticipated that temporary right-of-way will be needed along the corridor to construct the proposed improvements. Final right-of way impacts will be assessed further as the design of the project continues.

Utility Impact

Existing utilities throughout the project limits comprise of overhead electric, overhead telephone, underground telephone, water, sanitary sewer, storm sewer, and natural gas pipeline. No disturbance to the sanitary sewer is anticipated; however, disruptions to other utilities are expected. Utility coordination will be conducted throughout the design of the project to determine and assess appropriate dispositions of impacted utilities.

Traffic Maintenance

In discussions with the city, a full closure of Washington Avenue has been discussed for the proposed project to facilitate the roadway reconstruction. Detour routes will be provided for all traffic and will be further developed as the design of the project continues.

This Abbreviated Engineer's Assessment has been prepared by:

Dan Sherman, PE



March 31, 2023

American Structurepoint, Inc.
9025 River Road
Indianapolis, Indiana 46240

Concur:

John Sprague, PE



City Engineer, City of Vincennes

April 13, 2023

Date